

A Shared Vision for the Future

Liberty-Grand Street Heritage Corridor

Visioning Results and Recommendations

Newburgh, New York



Table of Contents

Prepared for:
The City of Newburgh, New York

Prepared by: A. Nelessen Associates, Belle Mead NJ
Visioning Planning Community Design



Proposed Goals of the Vision Plan	3
Overview of Process	4 – 6
Survey Demographics	7
General Characteristics of Corridor and Newburgh	8
What People Want: Overview	9 – 10
What People Want:	
Street Type Options	11 – 16
Pedestrian Realm Options	17 – 21
Development Options: Commercial/ Mixed-use	22 – 24
Development Options: Residential	25 – 29
Signage Alternatives	30 – 31
Fencing/ Semi-public Edge Options	32 – 33
Parking Options	34 – 36
Parks and Open Spaces Options	37 – 39
Transportation Options	40 – 42
Existing Conditions Synthesis Map of Corridor	43
Susceptibility to Change Synthesis Map of Corridor	44
Recommended Streetscape Synthesis Map of Corridor	45
Vision Plan of Corridor	46 – 47

PROPOSED GOALS

Liberty – Grand Street Heritage Corridor Vision

Create an innovative plan for pedestrians, parking and transportation along the Liberty – Grand Street corridors.

Create an equitable plan where every existing property owner, business and resident benefits from any future redevelopment activities.

Create a development plan that maximizes the public input and minimizes conflict.

Create economic development potential.

Create a plan that maximizes the land potential of the area while contributing to the tax base of the City.



The Future
belongs to those
who can **Visualize It.**

The **Visual Preference Survey™** will help determine the character, scale, and quality of **Revitalization and Redevelopment** appropriate for the Liberty – Grand Street Heritage Corridor in Newburgh.

Liberty – Grand Street Heritage Corridor VPS & Workshop

Overview

The Visual Preference Survey™ (VPS) and accompanying questionnaire are unique tools developed by A. Nelessen Associates, Inc. to help a community envision and plan for its future. It is a process by which the public becomes the planners and the architects for their future.

A VPS was commissioned for and uniquely developed to be applied to the Liberty-Grand Street Heritage Corridor area in Newburgh. The results provide guidance for future planning actions and design standards for the City. It is a critical guiding document, with images that can be directly incorporated into any future planning recommendations. The VPS is typically the first step in the planning process; it garners public input and participation to make plans both more democratic and market-responsive.

The recommendations from this survey are generated from a careful analysis of the image results and image content, as well as the experience of hundreds of these surveys in other locations with similar issues. A review of the highest and lowest rated images reveals the characteristics of place which should be encouraged and likewise avoided along the corridor in the future. Policy recommendations are drawn from an analysis of all image results and the questionnaire and provide specific recommendations for the future.

The survey results are based on the principle that the higher the statistical rating of each of the images in the survey, the more appropriate its characteristics of place are for both the study area and the City's future health and prosperity. Conversely, the more negative the rating of an image, the more damaging its characteristics of place are for future prosperity and quality of life within the city.

Negative and inappropriate image characteristics inevitably reduce the potential economic value and quality of life of the places they depict. The negative images show the opportunity and direction for major redevelopment, including condemnation and rebuilding. The more negative the image value, the greater the need and potential for redevelopment. Positive images help provide the vision for future development.

Ideally, the entire perceptual experience of Liberty Street and Grand Street as corridors into and through Newburgh should be positive, that is, as you walk or drive the total experience should be perceived above +1 to be considered a successful place (please see page six for an explanation of the image values and their relationship to the appropriateness of the image

characteristics). Spaces that engender values between -1 and +1 should be avoided as they represent the middle ground compromise and places of mediocre quality. No negative images should be allowed to remain, or be built in the future, if a municipality wishes to achieve the goal of improved quality of life and economic vitality.

Responses from the Visual Preference Survey™ and community questionnaire provide an understanding of what participating members of the community envision as appropriate for their future. These images should guide the direction of future Master Plans or Redevelopment Plans. They are recommended as illustrations for the goals and objectives of various plans, as examples of potential development options and recommendations, but most importantly can be used to prepare specific engineering design and construction documents and can be used as illustration in a zoning ordinance.

It is critical to remember that Newburgh, as an older city, has continued to lose population, meanwhile, neglected downtown building stock along the Liberty and Grand Street corridors continues to deteriorate, although a considerable amount of the compromised building stock remains in at least salvageable condition. If areas along the Liberty and Grand Street corridor are given negative ratings, there is less future market appeal and the downward spiral of deterioration and marginalization will likely continue.

To reverse this trend, positive or potentially positive characteristics of place should be created and marketed along the corridor, taking advantage of both the existing building stock and historic attributes found along the corridor. This is critical to the future of both the study area and Newburgh's future as a city on the whole.

Successful communities have one thing in common – a vision of where they want to go.



Liberty – Grand Street Heritage Corridor VPS & Workshop



Following the VPS and questionnaire, the second phase of the visioning exercise for the Liberty-Grand Street Heritage Corridor Study, the Vision Translation Workshop, asked participants to identify the appropriate locations for potential streetscape revitalization and redevelopment along the corridor. Based on the Vision and policies established by the positively rated image results for the VPS, participants located the preferred characteristics and future recommended land use alternatives along the Liberty-Grand Street corridors.

The premise of the Vision Translation Workshop is twofold. First off, while the VPS demonstrates what characteristics participants want for the future, the exercises found in the Translation Workshop indicate where participants prefer to see those characteristics located. The second premise of the translation workshops is that members of the community generally have a good idea as to where different design solutions might be most appropriate as applies to areas they know, facilitating public involvement in the creation of the physical vision plan. The goal of the Vision Translation Workshop is to first verify or modify the existing study area conditions in order to develop and solidify a plan.

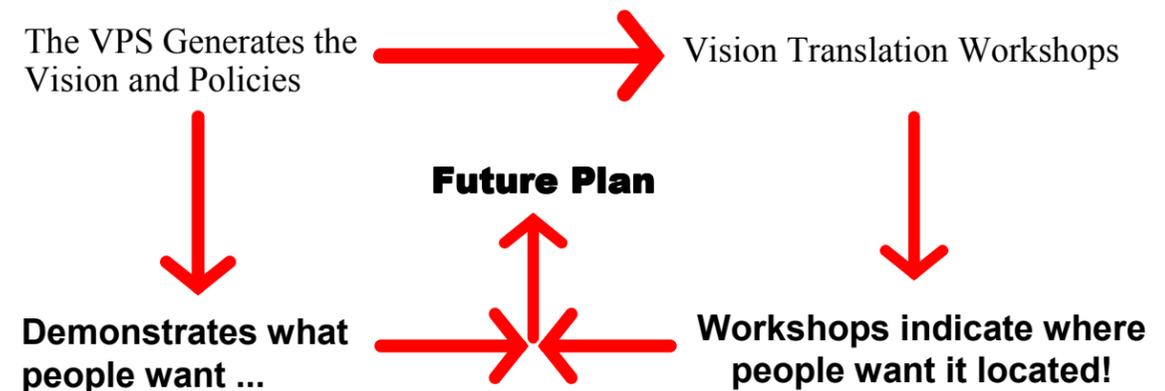
As the first task, participants were asked to identify the existing conditions along the two corridors. The existing conditions evaluation included identifying existing streetscape characteristics and their inter-relationships and boundaries.

The second task was developing an overlay for the corridor called a “Susceptibility to Change” map, which highlights the streetscapes and land uses along Liberty and Grand Streets most susceptible, moderately susceptible and least susceptible to change over time. This map is integral in identifying and prioritizing the areas where revitalization and perhaps redevelopment should be concentrated.

Following the Susceptibility to Change exercise, participants were tasked with identifying the most appropriate locations along the corridor for infill redevelopment as well as

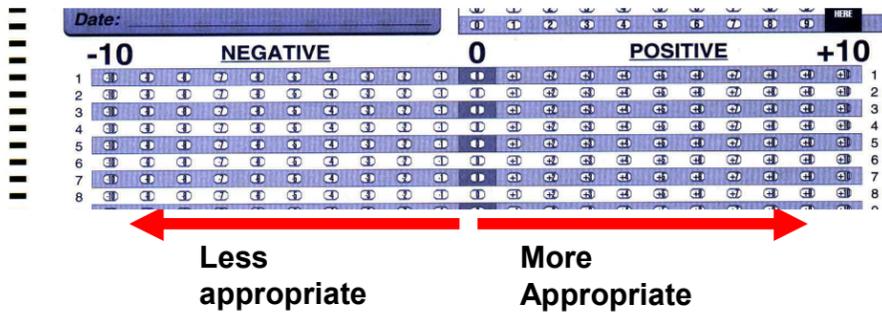
locations most appropriate for commercial and pedestrian activity. The final exercise of the Visual Translation Workshop had participants locating the most appropriate locations along the corridors for the various desirable streetscape characteristics as identified in the Visual Preference Survey.

A total of 114 members of the public participated in the Visual Preference Survey, questionnaire and Vision Translation Workshop held in February of 2006 at the Public Library in Newburgh. Following the VPS and Workshop, ANA staff, Larson-Fisher Associates staff and city officials synthesized all the input from the VPS and workshop participants groups to develop the consensus Vision for the corridor, which is found in this report.



Liberty – Grand Street Heritage Corridor VPS & Workshop

Each image was rated from between +10 to -10 based on the question, "How appropriate or inappropriate is the image you are seeing for the Liberty-Grand Street Corridor now and in the future?"



Negative images have the greatest potential value.

Positive images have the greatest market value.

How to read the Image Results

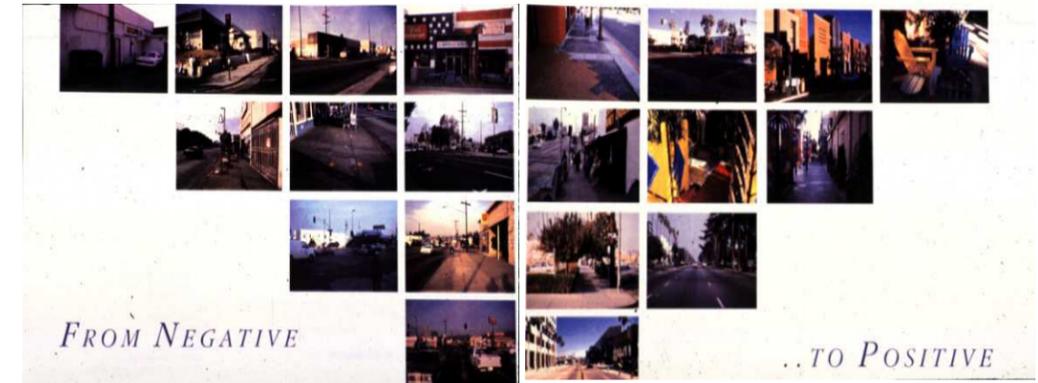
Images were evaluated with two statistical values, the mean (or average) value of the image and the standard deviation, a measure of the range of opinion values. The larger number on each image is the mean value of the image, while the number in parenthesis is the standard deviation. To determine the approximate range of value responses to an image the standard deviation is both added and subtracted from the mean value. As an example, the range of values responses would be -1 to +7 for an image having a mean of +3 with a standard deviation of 4.

Image values can be divided into three main categories:

- Category One:** Values ranging from +2 to +10
Positive images indicate places that people think are the most appropriate and desirable. The greater the positive value given to an image the more appropriate for the future. Implementation of the positive images enhances marketability and desirability.
- Category Two:** Values ranging from -1 to +1
This category represents the most mediocre of results. The typical response is that this is: "as good as we can expect" or "better than what was there". Images in this category can go positive or negative depending on their future treatment
- Category Three:** Values below -1
Negative images create apathy, detachment, and a depressive state of mind. They also provide the greatest opportunity for increases in value. If the places they represent are appropriately redeveloped, as shown in the positive images, significant changes in the value of the places they represent will occur.

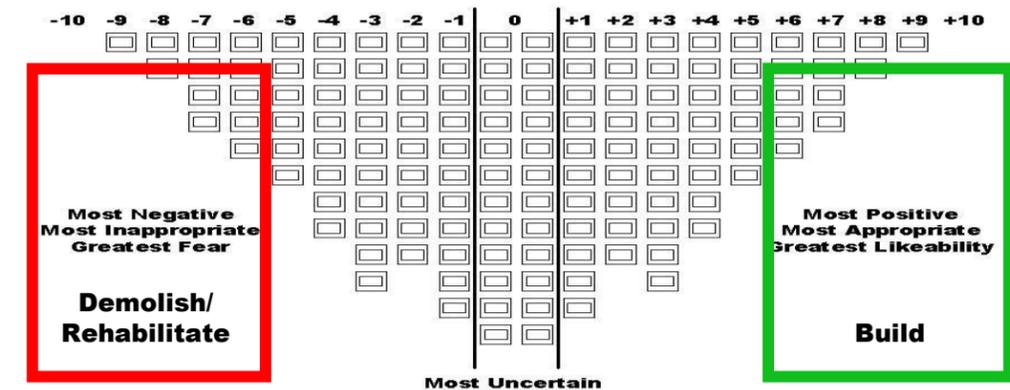
The lower the standard deviation between values, the greater the agreement between participants as to the value of the characteristics found in the image. Images with high mean values and low standard deviations provide a clear direction for the future of the corridor and City as a whole. The most critical are images with values that fall between the -1 and +1 range. Typically these images illustrate the most compromised locations while participants have become desensitized to their marginal quality.

The intensity of negative or positive reactions to the various images provides direction for future planning, zoning, development and redevelopment. Images which received the highest positive value ratings in a single category were reviewed and awarded the highest priority in the future vision for the corridor.



The Genius Loci - The peculiar character of a place with reference to the impression it makes on the mind...

It is critical to understand that the strength of the emotional reaction to each of the images in the various categories provides the measure of appropriateness. The higher the perceived image value, the more important the characteristics in the image are to maintain the quality and character of place, to maintain the quality of land, to improve the wealth of the City and its citizens, to improve its competitive advantage among surrounding municipalities, to improve the quality of life, promote and achieve sustainability and to make people proud and content to live and work here.



Demographics of Visioning Participants

A total of 114 members of the public participated in the Visual Preference Survey, questionnaire and Vision Translation Workshop held in February of 2006 at the Public Library in Newburgh.

The largest percentage of participants in the Liberty-Grand Street Corridor Visioning Process, at 38%, were born between 1945 and 1956. The next largest cohort of participants at 22% of the survey were born between 1957 and 1967. Those participants born before 1945 made up 19% of the survey while those born between 1968 and 1980 made up 18% of the survey. Those born after 1980 made up 4% of the survey.

The survey was nearly divided by female and male participants, who accounted for 49% and 51% of responses respectively.

Participants who have completed a graduate degree accounted for the largest percentage of survey participants at 39% while participants who have completed an undergraduate degree accounted for 27% of the survey. Participants who have finished some college or attained an associates or technical degree accounted for 19% of the survey while those with a high school diploma made up 14% of the survey.

The largest percentage of participants, at 23%, have household incomes ranging from \$100,000 to \$149,999. Household incomes ranging between \$50,000 and \$74,999 accounted for 19% of the survey while incomes ranging from \$35,000 to \$49,999 accounted for 16% of the survey. Participants with incomes between \$75,000 to \$99,999 accounted for 11% of survey participants, while participants with household incomes above \$150,000 accounted for 14% of the survey while those with incomes below \$35,000 accounted for 17% of the survey.

The majority of participants (58%) in the survey reside in single-family detached housing while 20% of participants reside in a multi-family apartment building. Of the remaining participants, 11% reside in a townhouse while 5% live in a mixed-use building with ground floor retail and housing above.

While nearly one third of participants live on either Liberty Street or Grand Street, 22% of participants live on Liberty and 8% live on Grand Street, 27% of participants live on a street or in a neighborhood adjacent to the Liberty-Grand Street corridor. 43% of survey participants either do not live in Newburgh or live beyond the study area and its adjacent lands.

Of participants in the survey, 78% own their residence while 60% have two to three people living in their household. 56% of participants attended the Visual Preference Survey and Translation Workshop as interested citizens while 46% of participants are business owners in Newburgh.

The largest group of participants (37%) have lived in Newburgh for more than 15 years. 19% of participants have lived in Newburgh for 5 years or less while 17% have lived in Newburgh for 6 to 15 years.

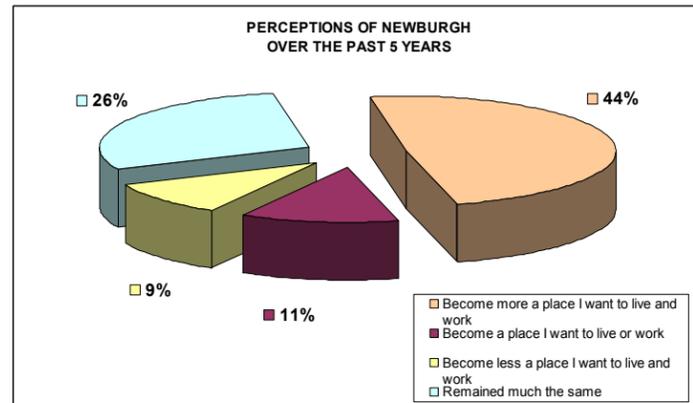
Most participants intend to reside in Newburgh for relatively long periods of time. The largest percentage of participants (33%) plan to live in Newburgh for the rest of their lives while 25% plan to live in Newburgh for at least the next 11 years. 13% of participants intend to remain in Newburgh from 6 to 10 years while 13% of participants plan to remain in the city for less than 5 years.

Of survey participants, 50% find themselves within the study area every day while 27% of participants find themselves within the study area a couple of times a week. 12% of participants only find themselves within the Liberty-Grand Street corridor study area on workdays while 10% of participants find themselves along the corridor just a couple of times a month or only during special occasions.

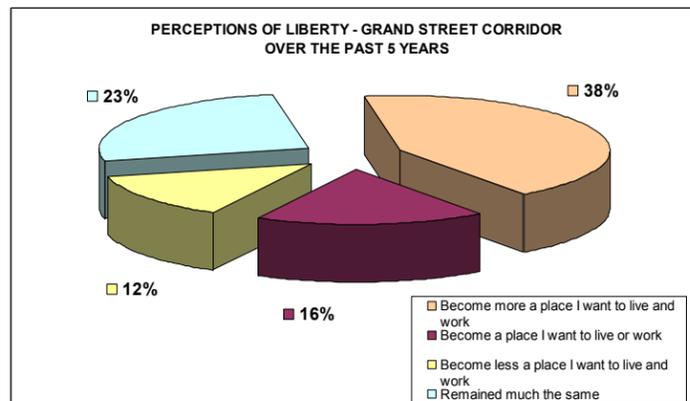
Liberty – Grand Street Heritage Corridor VPS & Workshop

General Characteristics of Newburgh

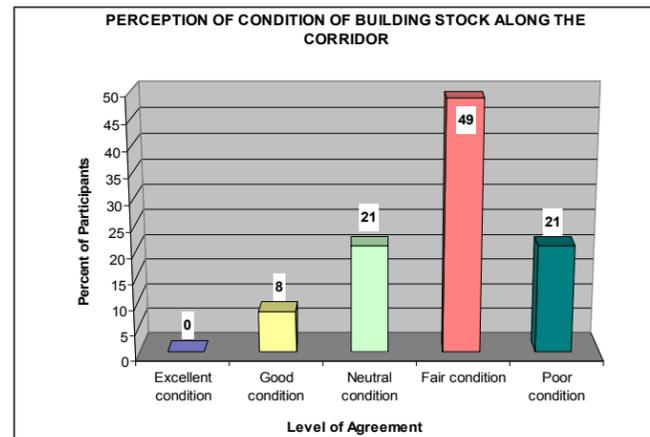
Indicating the positive direction in which Newburgh is heading, 44% of participants believe that in the last 5 years Newburgh has become more of a place that they want to live and work compared to 9% of participants who feel that in the last 5 years Newburgh has become less of a place they want to live and work. While 26% of participants feel that their perception of Newburgh has remained the same over the last 5 years, 11% feel that in the last 5 years Newburgh has become more of a place that they either want to work or live.



Reinforcing the priority of revitalization of the Liberty-Grand Street Corridor, 38% believe that the Liberty-Grand Street Corridor has become a more desirable place to live and work, while 44% of participants believe the city has become a more desirable place to live and work. Nearly the same percentage of participants (23%) believe that the Liberty – Grand Street Corridor has remained the same as those participants who believe that the City has remained much the same while a slightly higher percentage of participants (12%) believe the Liberty – Grand Street corridor has become less of a place they want to live or work.



Nearly a majority of participants considered the building stock along the corridor to be in generally fair condition. 49% of participants perceived the building stock to be in fair condition, with a **large** number of buildings and housing in disrepair and neglected, while 21% perceived the building stock to be in poor condition, with **most** buildings and housing in disrepair and neglected. Another 21% of participants perceived the corridor to have an equal number of buildings and housing in good condition compared to those in disrepair and neglect. 8% of participants believe the buildings and housing along the corridor is in good condition and well maintained while no participants believe the building stock along the corridor is in excellent condition.



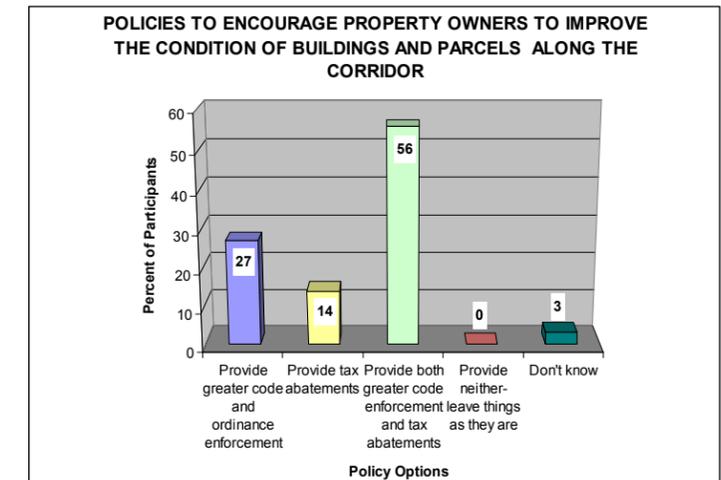
When asked to consider prioritizing potential revitalization actions for Liberty Street and Grand Street:

- 2% of participants indicated creating more open space as an immediate priority action for the corridor
- 2% of participants indicated creating new or improved existing neighborhood shopping nodes at major intersections along the corridors as the immediate priority action.
- 4% of participants indicated creating prominent signage promoting the historical significance of the “Heritage Corridor” as the immediate priority
- 7% of participants indicated improving the housing stock along the corridor should be the priority planning action.
- 38% of participants considered improving the physical characteristics of the corridor by cleaning and general maintenance of side-walks and street conditions as the corridor’s priority planning action.
- Nearly the majority of participants (47%) considered all the above as necessary immediate planning actions to revitalize the corridor.

100% of participants believe that deteriorated, poorly maintained or empty buildings detract from the economic value, marketability, and overall quality of the Liberty-Grand Street Heritage Corridor.

91% of participants support or highly support city-wide design standards to achieve and maintain better designed buildings, properties and landscaping and to protect the historic character of Newburgh.

Participants largely believe the City should pursue a range of policies to encourage property owners to improve the conditions of deteriorated and neglected buildings, parcels and housing along Liberty Street and Grand Street. A majority of participants (56%) believe the City should provide both greater code enforcement and ordinance violations and tax abatements as encouragement. Individually, 27% of participants believe the City should provide greater code enforcement while 14% of participants believe the City should provide tax abatements as encouragement.



Highest Rated Survey Image



What the People Want: Policy Decision

Increase Walk-ability

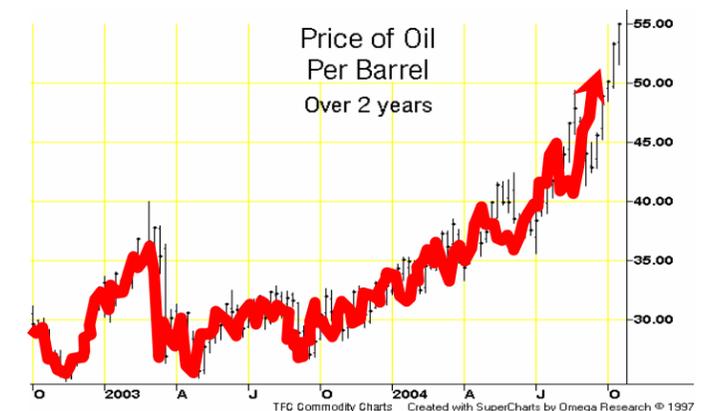
Enhance the pedestrian realm.

Expand parkways between sidewalk & street.

Use of textures on pedestrian surfaces.

Increasing the amount and interest of edges along public walkways.

“Get used to high Gas Prices” “most oil-producing countries and the major oil companies are producing all they can” NYTimes



Lowest Rated Images



The negative images have the greatest potential for value creation, if they are changed to the character and quality that are captured in the positive images.

What the People Want: Policy Decision **Maintenance and Cleanup as First Priority.**

Clean up! - Continue Current Cleanup Action.

Continue City's recent clean-up initiative & focus efforts on the Liberty – Grand Street Corridor.

Conduct a "Clean Sweep".

Hire area youths to maintain spotless environment – trash pick up & graffiti removal.

Visual Preference Survey Results

Category: Street Type Options

Streets are the most important public spaces of place

Liberty – Grand Street Heritage Corridor VPS & Workshop

Street Type and Streetscape Policy

Survey participants were asked to consider the quality and condition of Liberty Street and Grand Street. Almost unanimously, 91% of participants considered the condition of these “heritage” streets as unacceptable. Another 41% of participants perceived these streets as unacceptable but rated them as being in fair condition, with a large percentage of the street surfaces in disrepair and neglected. However, nearly a majority of participants (50%) perceived the streets to be in poor condition, with most of the street surfaces in disrepair and neglected.

When considering streetscape characteristics, participants largely value aesthetics and safety over convenience. A majority of participants (64%) indicated the most important streetscape characteristic is visually attractive streets while 28% indicated that having a safe walking environment was most important to them as a streetscape characteristic. Regarding convenience, 5% of participants valued smooth traffic flow with little congestion as the most important characteristic while 1% valued convenient parking and 1% valued convenient public transit access.

Gateways define the entrance to spaces. As one enters the gateways to the Liberty – Grand Street Heritage Corridor from adjacent streets, 91% of participants consider having a positive “gateway experience” important or very important to the physical character, quality and economic viability of the Heritage Corridor.

Participants were asked to consider various design treatments for the streetscapes along the corridor and a number of the treatments offered as examples were considered very appropriate for the corridor. In descending order of their priority, 95% of participants consider the restoration of historic building facades and implementation of design standards for new infill buildings to be appropriate or very appropriate. 92% of participants consider extensive landscaping with street trees either by adding new trees and or maintaining existing street trees as appropriate or very appropriate. Improving the sidewalks by resetting the stone slabs and requiring all new sidewalks be of stone rather than concrete to match the historic character was considered appropriate or very appropriate by 83% of survey participants. 78% of participants considered as appropriate or very appropriate, restoring or replacing existing fences at property edges and/ or installing new fences in appropriate locations providing they conform to appropriate design standards. Finally, among the preferred design treatments for the corridor, 75% of participants considered improving the street surface by pulling up the macadam to expose the original brick streets and then resetting and replacing where necessary the original brick pavement as appropriate or very appropriate.

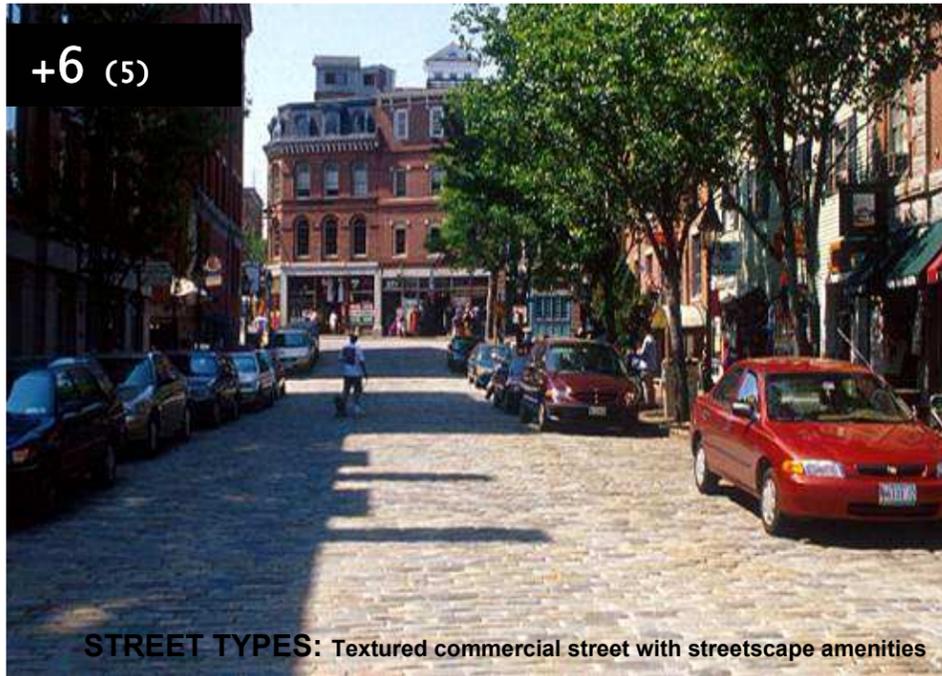
As a streetscape treatment one policy was more problematic for participants. Survey participants were nearly evenly divided regarding whether removing and replacing dilapidated, deteriorated and under-used buildings while relocating existing businesses to other locations. 44% of participants believe this is an appropriate or very appropriate policy treatment while 39% believe this is an inappropriate or very inappropriate treatment policy. 13% of participants were neutral. It is unclear as to whether participants were reacting to the removal of the deteriorated buildings or the relocation of existing businesses.

When asked to prioritize the streetscape treatments discussed above, the largest group of participants (38%) believe the first priority should be to implement design standards to improve building facades. 17% of participants believe that removing and replacing deteriorated buildings and relocating existing businesses should be the first priority, in slight contradiction to the division amongst participants as to the appropriateness of this as a streetscape planning action. Improving the sidewalks by resetting and replacing where necessary the stone slabs was considered the highest priority by 17% of participants while 14% of participants believe the macadam surface should be pulled up to expose the original brick street and reset and replace where necessary. Only 2% of participants considered restoring or replacing existing fences and installing new fences where appropriate as a first priority streetscape improvement.

The Liberty – Grand Street Corridor lies within a larger National Register of Historic District and contains a number of individually significant historic properties. Regarding the historical significance of the Liberty – Grand Street corridor, 94% of participants agree or highly agree that Newburgh should improve the historical sites along the corridor and provide better signage promoting the historical significance of the Heritage Corridor.



Liberty – Grand Street Heritage Corridor VPS & Workshop



Affirming the majority of participants (64%) who believe that visually attractive streets are the most important determinant of desired streetscape character, the positively rated images in the streetscape category have a combination of characteristics that have proven to be desirable as streetscape characteristics.

Valuable characteristics illustrated in these images include greening, in the form of street trees and textured pavements for streets and sidewalks. In addition, parallel on-street parking is located in all of these positively perceived images. Perhaps the most valuable feature found in these images, however, is maintenance. The streets, buildings and the general streetscapes are well maintained in these images.

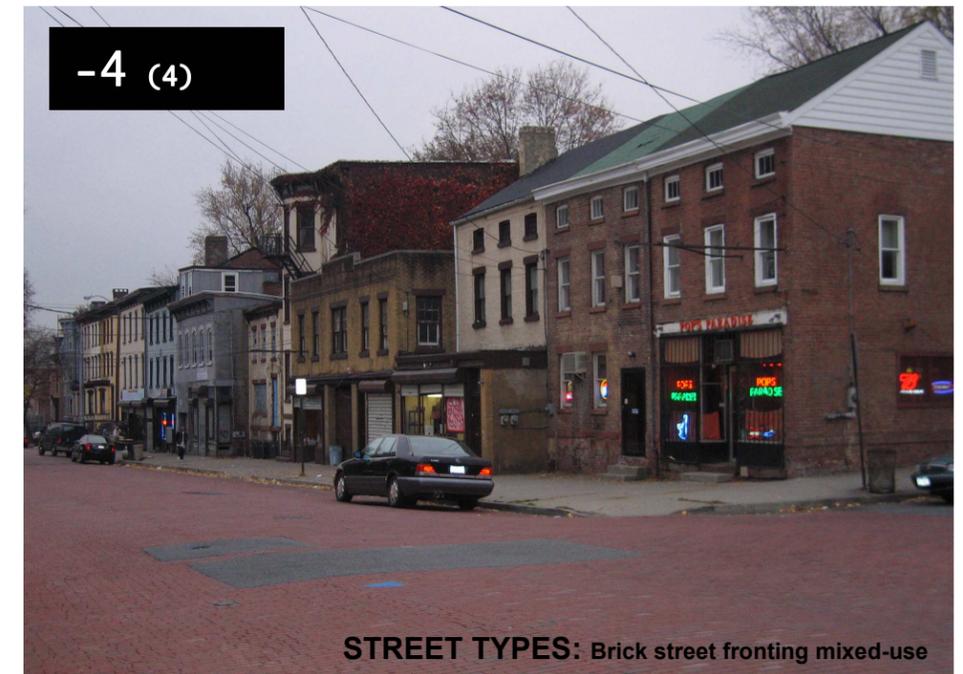
These characteristics work as interrelated components to create a safe walking environment, considered the most important streetscape characteristic by over a quarter (28%) of the survey participants and integral to a positive experience of place.

Desired Streetscape Character

64% Visually attractive streets

28% Safe walking environment

Liberty – Grand Street Heritage Corridor VPS & Workshop



Most images of existing corridor streetscapes evaluated in the survey received highly negative ratings, as indicated by the images on this page. The negative ratings of these images are supported by the overwhelming majority of participants (91%) who rated the corridor's condition as fair to poor, indicating a highly negative overall perception of the existing streetscapes of the Liberty and Grand Street Corridor.

The negative images on this page, however, have the greatest potential for future value creation along corridor streets. Restoring the “tired” look of these street to the vibrant streetscapes they once were will change the perception of the corridor from negative to positive and improve its value for both corridor residents and the city as a whole

A primary characteristic in these images contributing to the negative perception of the corridor is the deteriorated condition of the street surface, particularly the uneven pavement and repair patches of macadam amidst the historic brick pavement. In addition to the pavement texture, trash along streets, sidewalks and curbs highly contributes to the negative perception of place. Removing trash and keeping the streetscape clean will greatly improve the perception of the corridor.

Streetscape Character

91% of participants rated the quality and condition of Liberty and Grand Streets as either fair or poor.

What the People Want: Policies

Street Types and Streetscape Characteristics



Make Street Improvements

Revise Street Sections to narrower street width on Liberty and Grand Streets

Replace old infrastructure

Repave streets to design standards; maintain consistent street pavement

Reset stones on all corridor sidewalks

Reset granite curbs

Add street trees

Add cross walks

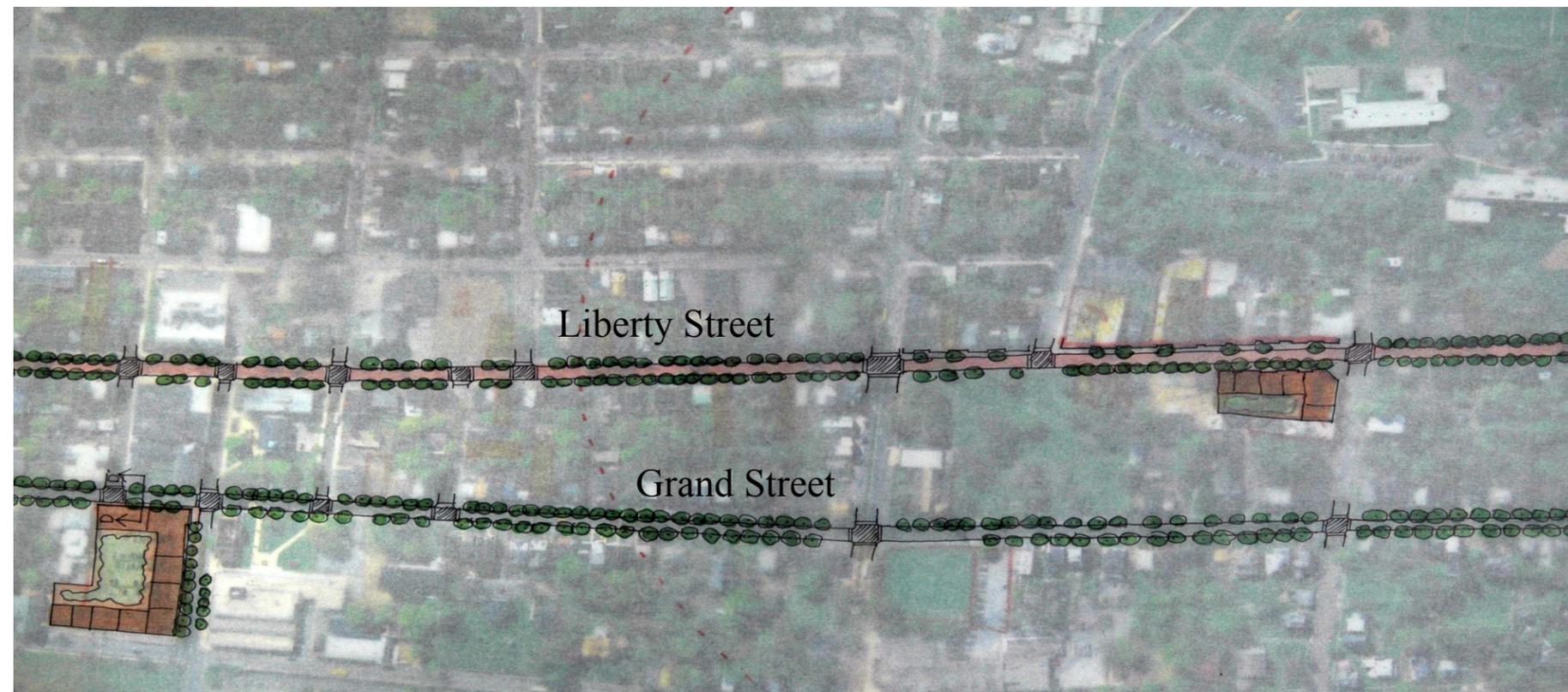
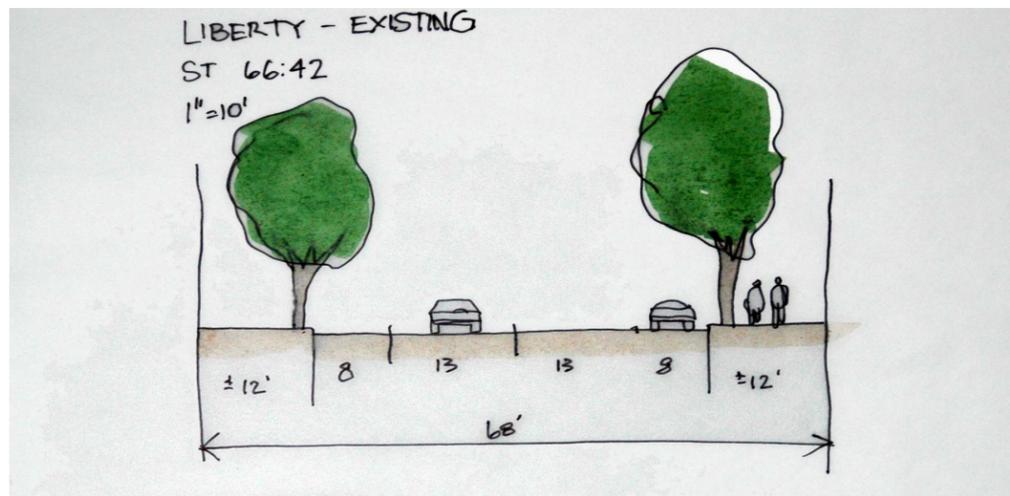
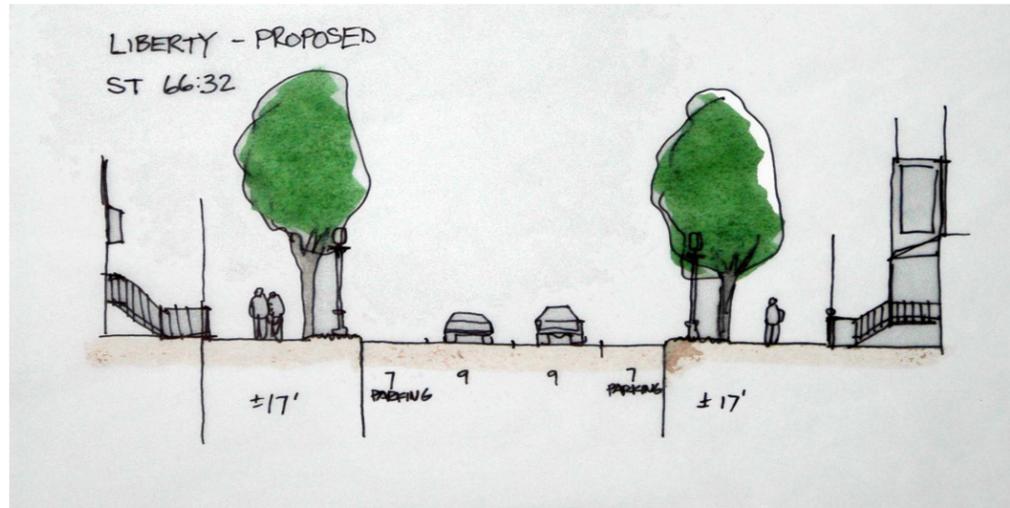
Add decorative lighting

What the People Want: Policies

Street Types and Streetscape Characteristics

Return Liberty Street & Grand Street to 2-way streets

Conduct a traffic study that re-evaluates the effectiveness of all intersecting 1-way streets & considers returning them to 2-way traffic



Visual Preference Survey Results

Category: Pedestrian Realm Options

Pedestrian realms are the spaces which define the pedestrian experience.

Liberty – Grand Street Heritage Corridor VPS & Workshop

Pedestrian Realm Policy

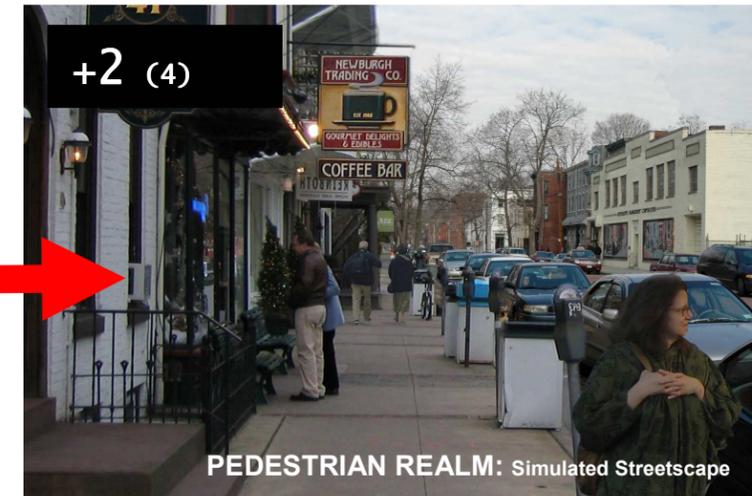
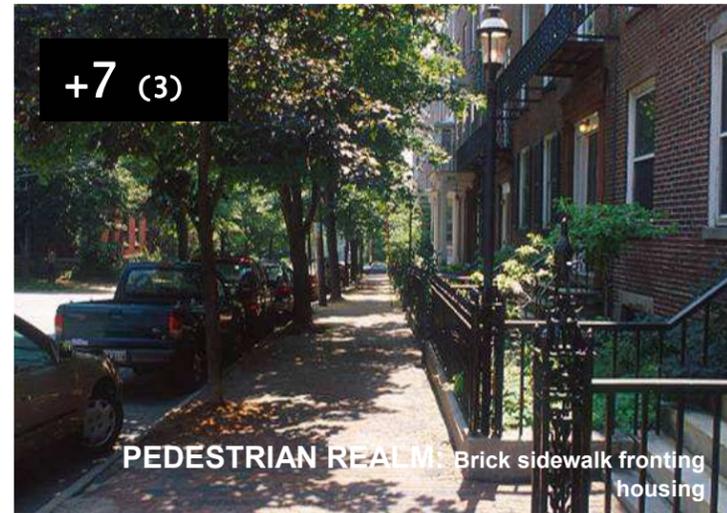
Survey participants were asked to consider the quality and condition of sidewalks along Liberty Street and Grand Street. A large majority of participants (88%) considered the condition of sidewalks on Liberty and Grand Streets as unacceptable. While 35% of participants perceived the sidewalks to be in fair condition, with a **large** percentage of the sidewalks and crosswalks in disrepair and neglected, a majority of participants (53%) perceived sidewalks and crosswalks to be in poor condition, with **most** sidewalks and crosswalks in disrepair and neglected.

The majority of participants (64%) generally consider the sidewalks to be of adequate width and adequately located. 20% of participants considered sidewalks a small inconvenience, located mostly where they should be although multiple pedestrians cannot pass each other comfortably. 14% of participants considered sidewalks inadequate.

Research has shown that the larger the presence of pedestrians the lower the incidence of crime and the safer the street. Participants tend to agree- 96% of participants think that more residents and pedestrians on Liberty and Grand Streets would make or may make the streets safer by providing “eyes on the street”.

Participants were asked to consider various pedestrian realm improvements and rate how appropriate they would be for the Liberty – Grand Street Corridor. Corroborating the value of “clean streets” as the highest priority pedestrian realm improvement, 96% of participants believe the clean-up and repair of sidewalks, increasing sidewalk width where possible and improving sidewalk and crosswalk character with design standards, is an appropriate or highly appropriate pedestrian realm improvement.

93% of participants consider the addition of street furniture including benches, trash receptacles, planters and banner poles where possible is an appropriate or highly appropriate improvements. Lighting is an integral component of safe pedestrian realms. The importance of pedestrian scaled lighting is understood by 83% of participants, who believe that pedestrian scaled lighting, which illuminates sidewalks and other pedestrian areas, as well as the street surface, is an appropriate or highly appropriate improvement for the pedestrian realm. Focusing on commercial areas of the corridor, though not considered as integral to the pedestrian realm as clean streets, street furniture and lighting, 75% of participants believe adding sidewalk pedestrian generators, improvements which attract pedestrians to a space, are an appropriate or very appropriate improvement for the pedestrian realm.

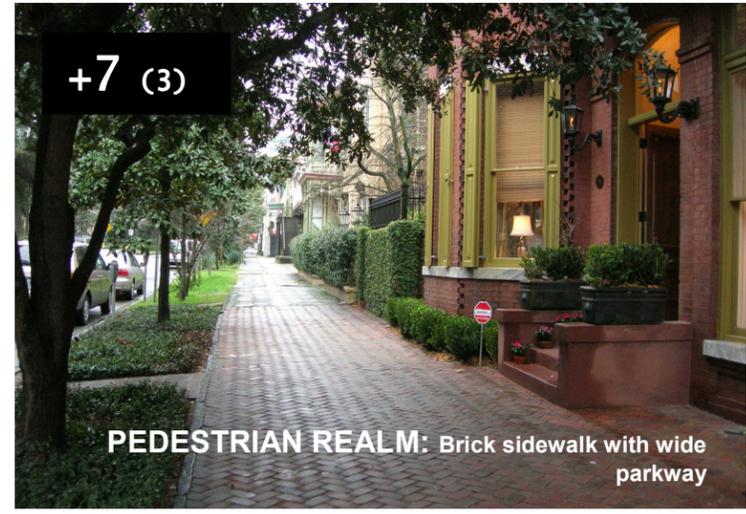


Liberty – Grand Street Heritage Corridor VPS & Workshop



+7 (3)

PEDESTRIAN REALM: Brick sidewalk fronting housing



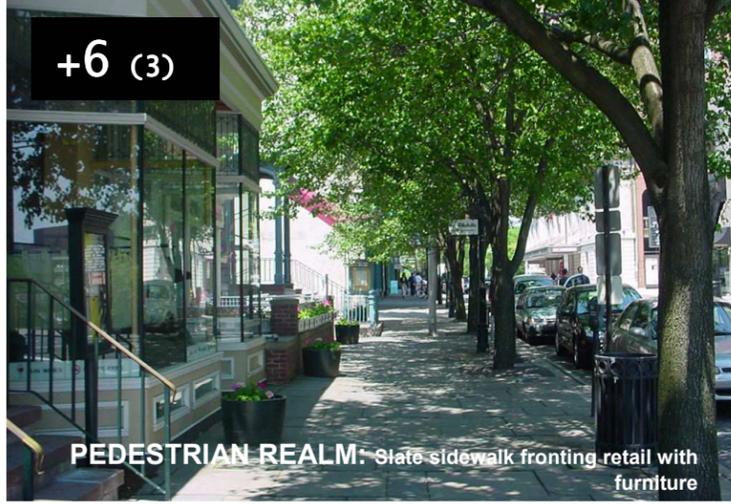
+7 (3)

PEDESTRIAN REALM: Brick sidewalk with wide parkway

What the People Want Policy Decision

As indicated by the highly rated positive images on this page, the pedestrian realm along the corridor requires upgraded sidewalks and semi-public edges for both residential and commercial sections of the corridor. The images reflect the beliefs of 96% of participants who feel that the clean up and repair of sidewalks along with improving sidewalk and crosswalk character with design standards is integral to the value of the pedestrian realm along the corridor. These images illustrate the value of maintenance of sidewalks and value of design standards.

Highly appropriate pedestrian realm and streetscape characteristics illustrated in these images include street furniture such as benches, planters, retail spilling out onto the sidewalk, chairs and tables fronting cafes and restaurants. The sidewalks in the majority of images are textured, also considered appropriate. The existing sidewalk stones, while they are uneven and have heaved over time, remain in very good condition. Sidewalk stones along the corridor should be reset or replaced with new stone.



+6 (3)

PEDESTRIAN REALM: Stone sidewalk fronting retail with furniture



+6 (4)

PEDESTRIAN REALM: Brick sidewalk with furniture



+6 (5)

PEDESTRIAN REALM: Brick sidewalk fronting housing



+6 (4)

PEDESTRIAN REALM: Mixed-use brick sidewalk with cafe furniture



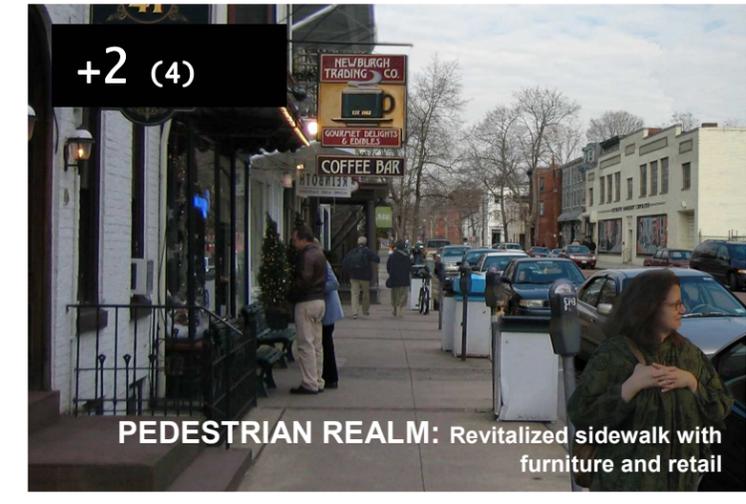
+5 (4)

PEDESTRIAN REALM: Brick sidewalk with furniture



+4 (4)

PEDESTRIAN REALM: Brick sidewalk with wide parkway



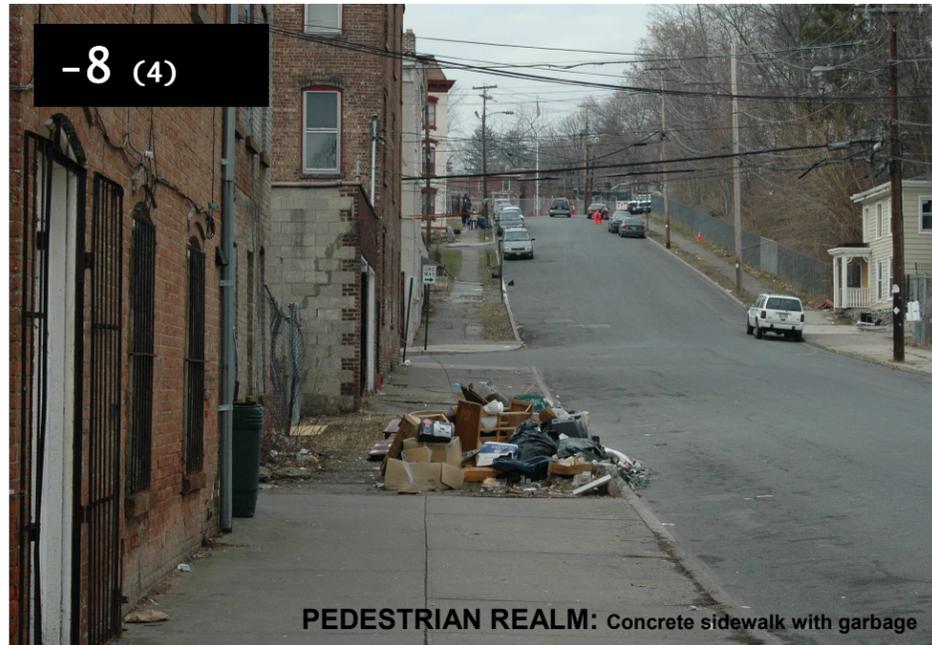
+2 (4)

PEDESTRIAN REALM: Revitalized sidewalk with furniture and retail

Stone sidewalks should not be replaced with concrete or any other material which does not conform to the historic nature of the corridor and to an established set of design standards if an improved appearance is to be achieved.

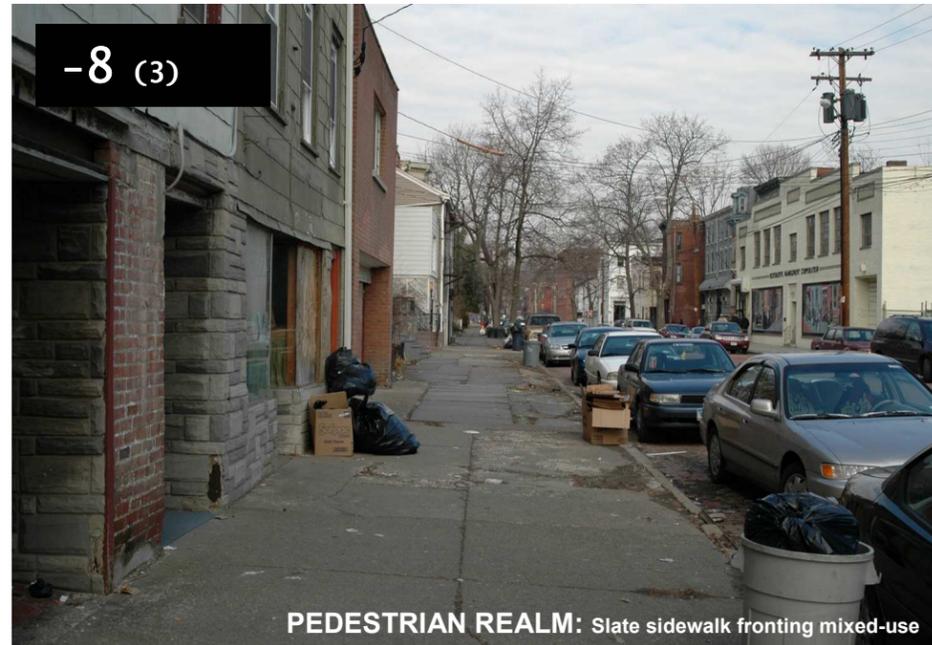
In addition to the quality and condition of the sidewalk condition and streetscape furniture in these images, commercial and residential building frontages and the semi-public edge along residential frontages in the highest rated images are well-maintained. The value of maintenance to the perception of the corridor can not be highly stressed enough.

Liberty – Grand Street Heritage Corridor VPS & Workshop



-8 (4)

PEDESTRIAN REALM: Concrete sidewalk with garbage



-8 (3)

PEDESTRIAN REALM: Slate sidewalk fronting mixed-use



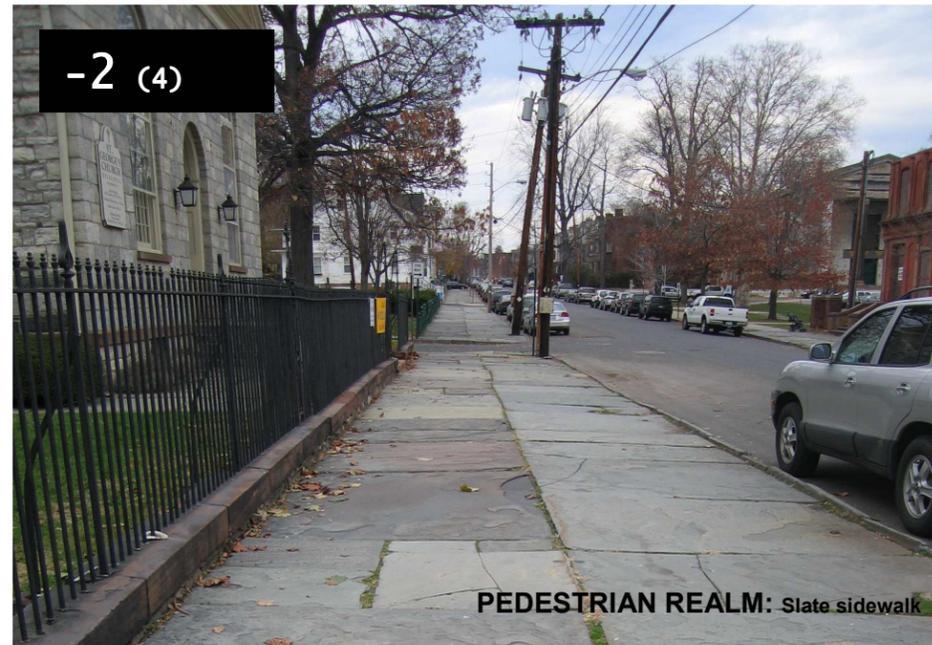
-7 (3)

PEDESTRIAN REALM: Slate sidewalk fronting housing



-6 (3)

PEDESTRIAN REALM: Concrete sidewalk



-2 (4)

PEDESTRIAN REALM: Slate sidewalk

83% Improve sidewalks by resetting the stone slabs – do not replace with concrete

The high negative ratings of the existing pedestrian realm images rated by participants correspond to the high majority of participants (88%) who considered the condition of sidewalks on Liberty and Grand Streets as unacceptable. The most noticeable characteristic in the most negatively rated images is the accumulated trash on the sidewalks. These conditions should be ameliorated immediately- an on-going maintenance program and strict enforcement of code violations should be used to prevent these conditions from occurring in the future at all cost.

Like the positive values on the preceding page, the negative values of these pedestrian realms corroborating the value of “clean streets”, as the highest priority pedestrian realm improvement, where 96% of participants believe the clean up and repair of sidewalks is an appropriate or highly appropriate pedestrian realm improvement.

In addition, while the sidewalk slates are in good condition, as indicated in the negatively rated image to the above right, heaved and uneven sidewalk surfaces are highly unacceptable. Sidewalk stones should be reset and replaced where necessary to ameliorate this condition.

What the People Want: Policies

Pedestrian Realm Characteristics

Clean up sidewalks and institute an on-going maintenance program

Widen narrower sidewalks where appropriate.

Replace old infrastructure.

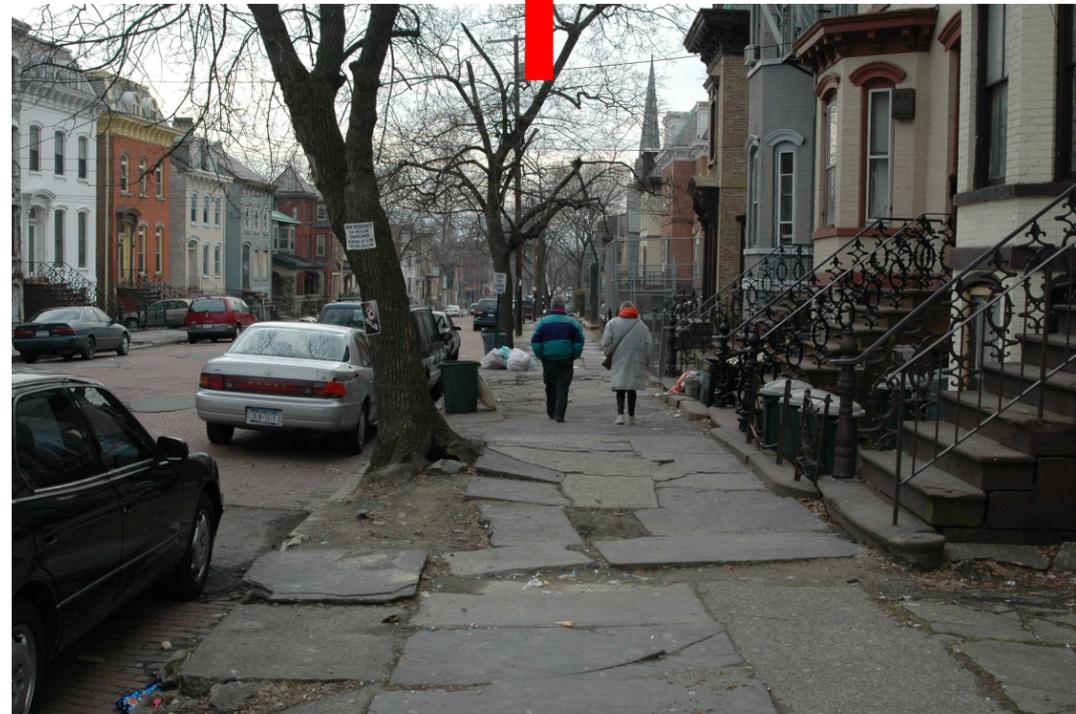
Reset stone slabs on sidewalks.

Reset granite curbs, replace deteriorated curbs with new granite.

Add new street trees and replace where necessary; trim and maintain existing street trees.

Add textured crosswalks with brick or stamped blacktop.

Add decorative pedestrian scaled lighting, keeping with the historic character of the corridor.



Visual Preference Survey

Category: Development Options: Commercial/ Mixed-use

Liberty – Grand Street Heritage Corridor VPS & Workshop

Commercial and Mixed-Use Development Policy

The Liberty – Grand Street Heritage Corridor contains a substantial percentage of deteriorated buildings. 100% of participants agree that deteriorated, poorly maintained or vacant buildings detract from the economic value, marketability and overall quality of the Corridor.

The Corridor also contains a number of vacant buildings and parcels and surface parking lots. 73% of participants believe the City should encourage building infill along both Liberty Street and Grand Street where there are gaps, surface parking lots or vacant parcels, between existing buildings.

A majority of participants (57%) agree that the Liberty – Grand Street Heritage Corridor is to valuable an asset to the City, given its proximity to the riverfront and downtown, for use as surface parking lots and single story buildings (underutilized land), and should be redeveloped at higher intensity.

Considering redevelopment along the corridor, 77% of participants believe infill of mixed-use buildings, with ground floor retail and housing or offices above, is appropriate.

When considering the value placed on the retail characteristics that are most likely to attract participants to a shopping experience, a majority of participants (59%) consider both the better quality of stores and services, greater variety of stores and services and convenience of stores and services as important. 22% of participants consider the quality of stores and services as the most important retail characteristic while 10% of participants consider the variety of stores and services as most important.

The following retail uses are recommended by participants along the Liberty – Grand Street Corridor according to the percentage of participants who favored the retail use:

- Locally owned businesses including restaurants and outdoor cafes, art galleries, antique stores, independent hardware stores and pharmacies and office supply stores were favored by 91% of participants.
- Bookstores, coffee shops and newsstands such as Borders, Starbucks, and locally owned shops located along the corridor were favored by 83% of participants with 11% of participants undecided.
- Garden and flower shops and local services including barber shops, beauty salons and banking located along the corridor were favored by 80% of participants.
- Professional and personal services including doctors, dentists, health clubs, salons, spas, dry cleaning, financial services and real estate located along the corridor were favored by 77% of participants.
- Specialty retail, specialized and small scale, and farmers markets located along the corridor were favored by 72% of participants and 71% of participants respectively.

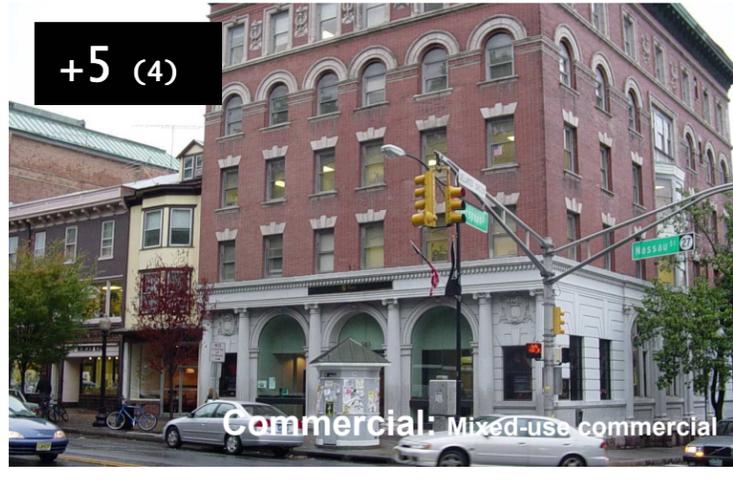
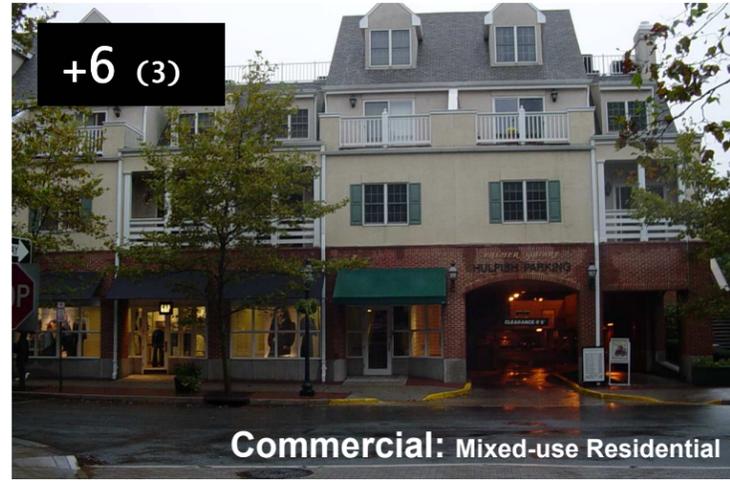
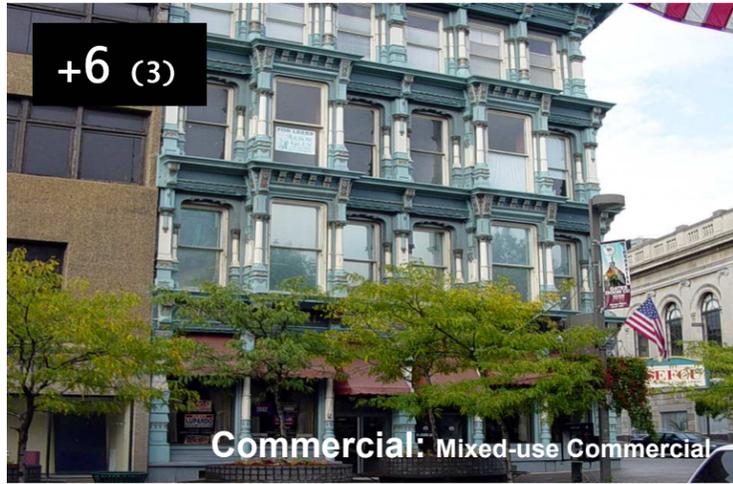
A majority of participants (69%) do not support fast food and convenience retail including McDonalds, Subway, Burger King, Seven-Eleven located along the corridor.



100% of participants agree that deteriorated, poorly maintained or empty buildings detracts from the economic value, marketability and overall quality of the Corridor



Liberty – Grand Street Heritage Corridor VPS & Workshop



Policy Decision – Commercial infill.

The highly rated commercial and mixed-use building images rated in the survey illustrate the characteristics considered appropriate for future development along the corridor. The value of these images are supported by 77% of participants who consider mixed-use buildings with ground floor retail and housing above the most appropriate commercial development type for the corridor and 73% of participants who believe the City should encourage building infill along both Liberty Street and Grand Street where there are gaps, surface parking lots, or vacant parcels between existing buildings.

These development images reflect both the historic character of the corridor and suggest the direction of appropriate design standards to guide future infill and redevelopment, including bulk, height and design. Design standards should take the historic character of the corridor into account and address commercial frontages as well as all facets of building design. Reflecting the diverse history and diversity of building types within the corridor, the positively rated images indicate the range of building types participants consider appropriate for the corridor. In addition, note that the buildings illustrated in these images are well-maintained, along with their associated pedestrian realm frontages.

Visual Preference Survey

Category: Development Options: Residential

Liberty – Grand Street Heritage Corridor VPS & Workshop

Residential Development Policy

The potential value of a revitalized Liberty – Grand Street Heritage Corridor is indicated by a majority of participants, 68% of whom would be interested in living on the corridor if there was high-quality housing which was affordable to a range of household incomes.

Municipalities must provide low and moderate income housing within some percentage of the overall households in a community in part to accommodate the housing needs of the service sector. The largest percentage of participants (32%) believe a ratio of 80% market rate housing to 20% moderate and low income affordable housing is the most appropriate mix for the Liberty – Grand Street Corridor. 27% of participants believe a ratio of 60% market rate housing to 40% moderate and low income affordable housing is the most appropriate mix for the Liberty – Grand Street Corridor while 15% of participants believe the ratio should be 50/50.

According to survey participants, the most appropriate type of new housing redevelopment along the corridor is mixed-use development, with housing on upper floors above ground floor retail or services. 77% of participants consider mixed-use housing along Liberty and Grand Streets as desirable. 60% of participants consider multi-family residential buildings as appropriate and desirable for the corridor while 57% of participants

consider townhouses, at least two single family housing units attached to each other, as appropriate and desirable housing typologies for the corridor. Given the pattern of densities of development along the corridor and its proximity to the downtown, it is not surprising that single-family housing was only considered appropriate and desirable along Liberty Street and Grand Street by 46% of participants.

The highly rated residential images on this page indicate a clear direction for future residential infill, redevelopment and revitalization. As with other components of the perception of place, these images all represent well-maintained residential structures. On-going maintenance is paramount to improving and protecting the aesthetic value and market value of residences along the corridor. These images illustrate the value of significant greening and healthy, maintained trees fronting residences and the well-defined semi-public edge separating yards from the sidewalk. In addition, residences are elevated above the sidewalk, providing an increased sense of security.

The high values of these images also suggest the direction of design standards for residential buildings along the corridor. In addition to being elevated above the sidewalk and the semi-public edge, these residences are primarily built of brick or masonry, reflecting the historical building materials used along the corridor. Characteristics including appropriate building materials, setbacks, elevation, greening and appropriate semi-public edge treatments should be addressed with design standards.

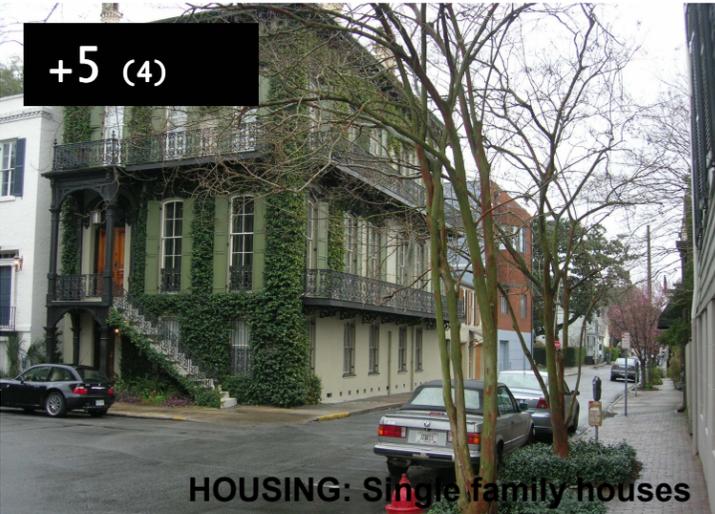
73% think that the City should encourage infill along the corridor where there are gaps between buildings and where there are currently parking or vacant lots.

Policy Decision – Residential Infill.

Promote Infill Residential and Mixed-Use Construction



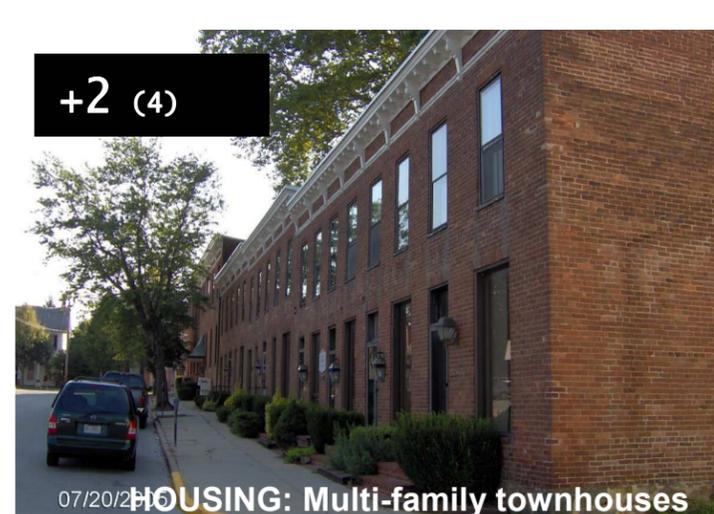
Liberty – Grand Street Heritage Corridor VPS & Workshop



Although participants considered multi-family housing (60%) and townhouses (57%) as more appropriate housing for the corridor than single-family housing (46%), the range of housing which rated positively in the survey indicates that participants consider a wide range of housing types is appropriate for the corridor. In short, a mix of residential types is indicated along the corridor, which continues the existing pattern of development.

All of the housing typologies on this page, as on the previous page, are more urban in nature and density, as opposed to suburban. Another telling characteristic in these images is the degree of maintenance of the buildings. The value of on-going maintenance can not be stressed enough as a fundamental determinant of the perception, and therefore value, of place.

Like the images on the previous page, characteristics found in these images contributing to their value include significant landscaping and greening and healthy, maintained trees fronting residences, well-defined semi-public edges separating yards from the sidewalk. In addition, most of the residential buildings in these images have ground floors elevated above the sidewalk, providing an increased sense of security.



Liberty – Grand Street Heritage Corridor VPS & Workshop



The negatively rated residential images above illustrate how the lack of maintenance and upkeep affects the perception of place. The significance of appropriate design standards is another characteristic illustrated by the top left image and lower right image. The top left image received a very high negative rating, indicating that value-engineered development designed with no consideration of its surrounding context, namely its location adjacent to a historic area and heritage corridor, is undesirable. Future development of this type is considered highly inappropriate by survey participants.

Located along the corridor, the image to the lower right is a new project. While this development addresses the historic context of the surrounding area with the building material, participants were evenly divided as to whether the actual design is appropriate for the corridor. In the future a strict set of design standards must address building materials as well as bulk and scale, the pattern of windows, and adjacent characteristics including sidewalks and semi-public edges.



What the People Want: Policies

Development Recommendations

Aggressively enforce code violations with property owners

Promote Infill Mixed-use and Residential Construction

Plan for the development of a substantial number of dwelling units in various locations throughout the corridor

Encourage mixed-use development with residential at specific locations along the corridor & along Broadway

Build a multi-use, centrally located parking structure in or adjacent to the corridor

Infill all surface parking lots & selective demolition sites with mixed-use and residential development

The Vision Plan calls for:

1,260 units @ 3 stories with retail/services on the ground level in specified locations

Two gateways towers 164 units

Total Units 1,428 units

Investment Potential @ \$300,000,000 to \$360,000,000 in new housing

Visual Preference Survey Results

Category: Corridor Signage Alternatives

Liberty – Grand Street Heritage Corridor VPS & Workshop



+6 (3)

SIGNAGE: Signage hanging on building front



+6 (3)

SIGNAGE: Way-finding & Informational signage



+5 (3)

SIGNAGE: Signage mounted on building front



+5 (4)

SIGNAGE: Way-finding signage



+5 (4)

SIGNAGE: Banner as gateway signage



+5 (5)

SIGNAGE: Mural on building front

Signage Policy

Appropriate signage is an integral component of a positive experience of place. Signage serves to both identify, direct and guide. Participants agree by a large majority (91%) that a positive gateway experience, including signage, is important to the physical character and economic viability of the corridor. From the positively rated signage images, participants favor as appropriate commercial signage which is traditionally urban – smaller scaled, pedestrian oriented signage hanging from building fronts and smaller scaled signage mounted on building fronts directly over the entrances. Participants also favored way-finding signage, in the image above right, here including bicycle facilities, and gateway signage in the form of banners and murals. Graffiti is perceived very negatively and a program to lessen incidents of graffiti and clean off new graffiti immediately should be a priority revitalization policy for the corridor.

What the People Want: Policies

Signage Characteristics

- Institute a clean-up policy whereby graffiti and vandalism are immediately removed or fixed following discovery.
- Institute design standards for commercial signage which dictate appropriate location, size, height, color and spacing
- Install way-finding signage throughout the corridor which identifies historic sites, parking and other places of interest.
- Install monument style signage at the corridor's gateways to signal entrance into the corridor.



-7 (4)

SIGNAGE: Graffiti on building



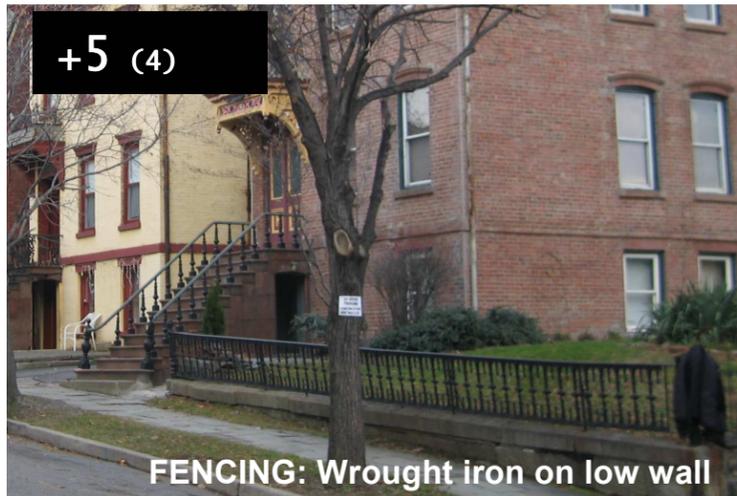
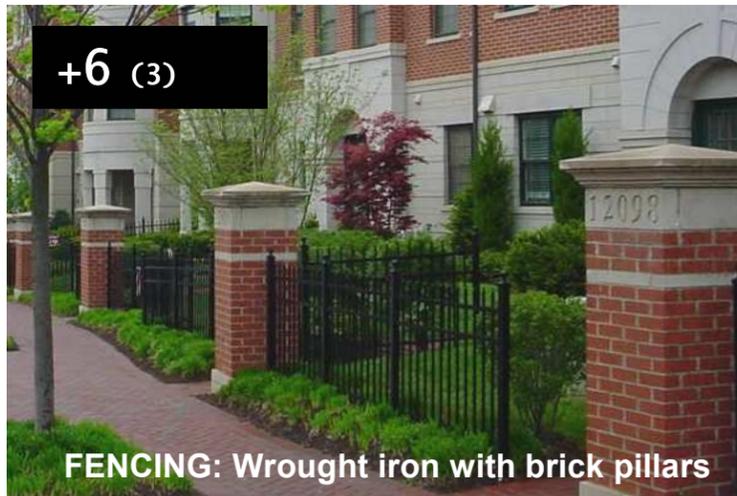
-1 (5)

SIGNAGE: Signage mounted on building front

Visual Preference Survey Results

Category: Fencing/ Semi-Public Edge Options

Liberty – Grand Street Heritage Corridor VPS & Workshop



78% of participants agree that existing fencing at property edges should be restored or replaced with fencing that conforms to design standards

The difference in values between the images of semi-public edge treatments to the left clearly recommend future policy for semi-public edge treatments.

The City must institute design standards for fencing and other variations of the semi public edge. Different types of fencing as the semi-public edge are appropriate for different residential types. The semi-public edge should also conform to the historical nature of the corridor.

Perhaps most importantly, the City must ardently enforce code violations for maintenance. Property owners must be held accountable for the condition and maintenance of the semi-public edges along their properties.

What the People Want: Policies

Semi-Public Edge Characteristics

Enforce code violations for maintenance

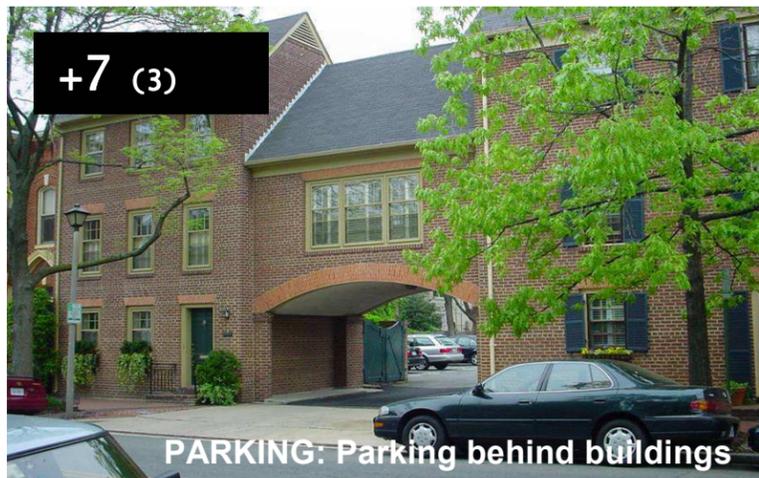
Design standards:

- Metal fencing with brick piers for townhouses and more urban housing
- Picket fencing for single family homes
- Prohibit all chain-link fencing along the corridor
- Prohibit solid fencing along street/sidewalk edges

Visual Preference Survey Results

Category: Parking Options

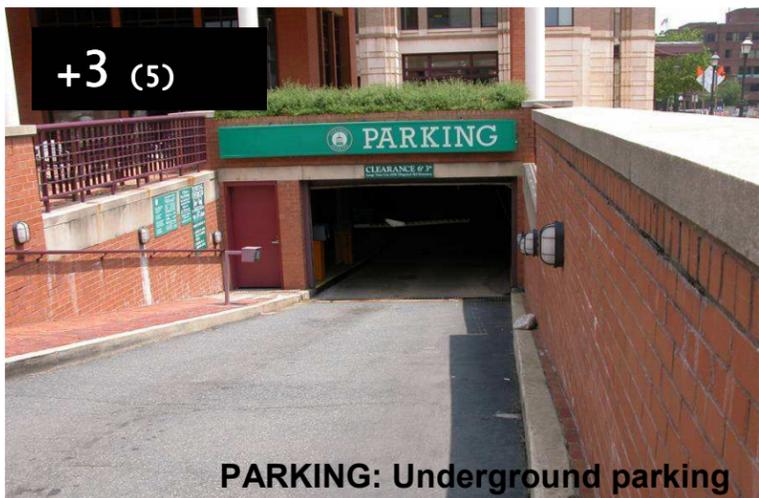
Liberty – Grand Street Heritage Corridor VPS & Workshop



PARKING: Parking behind buildings



PARKING: Mixed-use parking building



PARKING: Underground parking

Parking Facility Policy

The images in the parking category recommend a clear direction for future parking facilities along and adjacent to Liberty and Grand Streets. These images in part show the value to survey participants of both good and appropriate urban design.

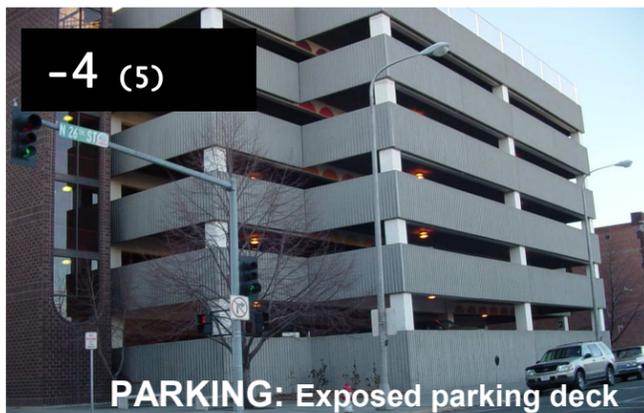
Indicated by the high +7 rating given by participants to the above left image, the preferred parking treatment along the corridor is to hide parking mid-block behind buildings lining the street front.

The difference in value between the exposed parking deck in the lower image to the left, which received a -4, and the mixed-use parking facilities in the above images, with ratings of +3, shows a clear recommendation for future parking facilities. Participants consider as appropriate parking either under buildings or in mixed-use parking structures where parking is located on floors above ground floor retail. Imperative to the value of the mixed-use parking structures were design standards which ensured that the parking facilities would be designed to screen parking or resemble commercial office space rather than parking.

What the People Want: Policies

Off Street Parking Alternatives

- Surface parking lots are the land bank of the future
- Promote mixed-use parking structures with ground floor retail
- Develop design standards to guide parking facility design
- Design standards should expressly prohibit exposed parking facilities



PARKING: Exposed parking deck

Ann Street Mixed-use Parking – 324 spaces per level
 3 levels @ 324 per level = 972 spaces





On-street Parking Policy

Images of on-street parking in the parking category indicate a clear, preferred direction for future on-street parking along Liberty and Grand Streets. As with the perception of off-street parking facilities, these images indicate the value to survey participants of both good and appropriate urban design relating to on-street parking.

Indicated by the high +4 ratings given by participants to these images, on-street parking treatments along the corridor should incorporate both diagonal and parallel parking where possible. A street regulating plan should specify pavement textures and materials consistent with the historic nature of the corridor. To match the historic pavement type and further “extend” the width of the sidewalk, parallel parking spaces should be textured and diagonal spaces should be bumped out with textured paving and street trees planted in the bump-outs.



What the People Want: Policies

On Street Parking Alternatives

On-street parking, whether parallel or diagonal, should be well designed.

Parallel parking spaces should be clearly designated and textured.

Diagonal parking should be located where possible with appropriate street widths.

Diagonal parking bumpouts should alternate between textured paving material and landscaped planters for street trees.

Require ongoing cleaning and maintenance of street and sidewalk surfaces.

Visual Preference Survey Results

Category: Parks and Open Spaces Options

Liberty – Grand Street Heritage Corridor VPS & Workshop

Recreation Parks and Plazas Policy

Open spaces are an integral component of the urban fabric and include parks, plazas and, in the case of the Liberty – Grand Street Heritage Corridor, also include cemeteries and historic sites. Most existing open spaces along the corridor evaluated in the survey, including vacant lots, were perceived negatively, as indicated on the bottom row of images. The green fronting the historic court house is a notable exception, having received a high positive rating of +6, along with the existing cemetery, which received a very low acceptable score of +2, most likely due to its deteriorated condition.

Although open space exists along the corridor, 75% of participants believe the corridor does not have an adequate number of parks and urban plazas to meet either the existing or future needs of the Liberty – Grand Street corridor communities. This indicates the value of parks and plazas to the positive experience of space and is especially significant given the historic nature of the corridor and the potential for increased exposure of the corridor to tourists.

The significance of the corridor as a historic resource is affirmed by 97% of participants who believe the various cultural and historic properties along the corridor should be improved and better promoted to attract visitors.

Integral characteristics of safe and successful urban open spaces, parks and plazas include good design and maintenance. As with other community characteristics, ongoing maintenance is one of the primary determinants of an image's acceptability by the survey participants, indicated in the images above. Likewise, appropriate and well thought out urban design must guide the planning of parks and plazas along the corridor.



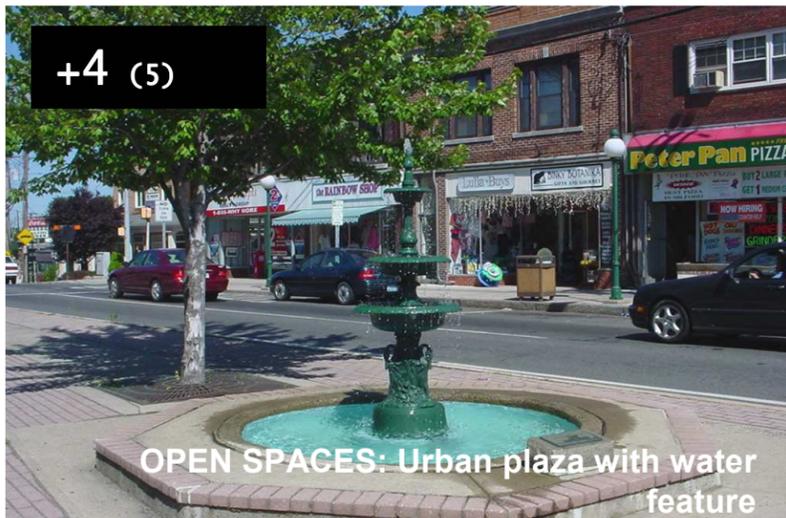
75% of participants do not think that the corridor has an adequate number of park and urban plazas



Liberty – Grand Street Heritage Corridor VPS & Workshop



OPEN SPACES: Urban plaza with furniture



OPEN SPACES: Urban plaza with water feature

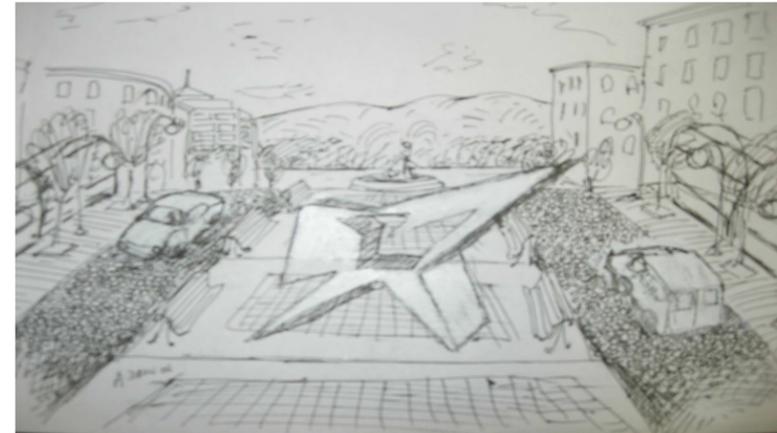
Link the Liberty-Grand Street Corridor with the riverfront via Broadway

Build an art bridge that is a *must* to walk across

Trolley/Tram line/Funicular

Grand Stairway/Escalator

What the People Want: Policy Decision



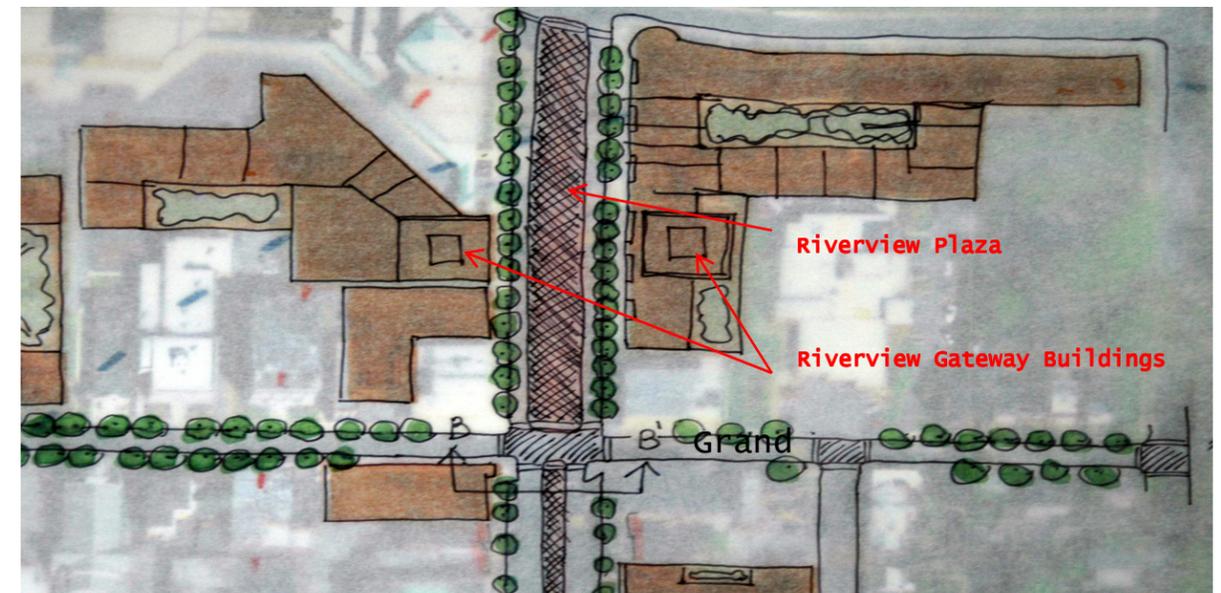
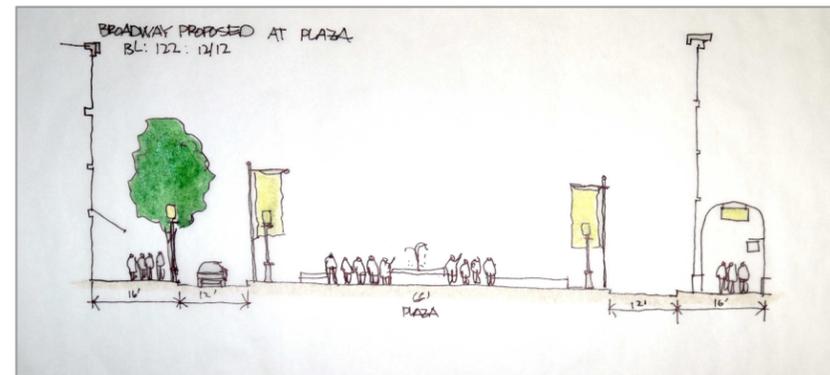
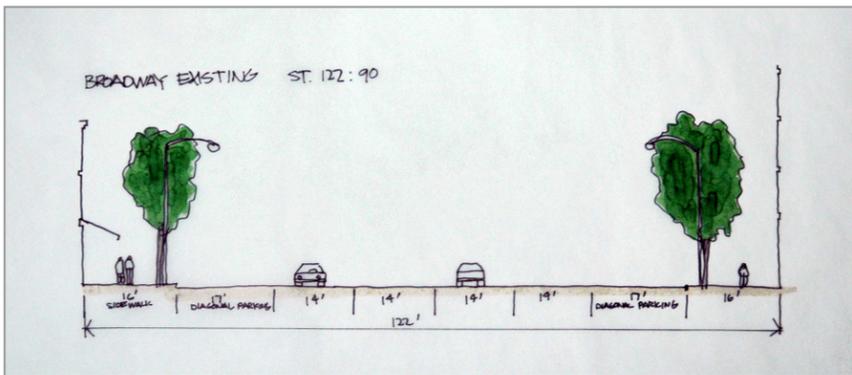
Create a destination to enhance the connection to riverfront.

Create a new “Riverview Plaza”/boulevard along Broadway from Landers Street to Marine Drive & riverfront.

Strengthen the presence of Washington’s HQ along Grand Street as a destination within the corridor.

Display large scale sculptures here to provide a destination for Dia & Storm King visitors.

Include water elements to strengthen the sense of connection to the Hudson River.



Visual Preference Survey

Category: Transportation Options

Liberty – Grand Street Heritage Corridor VPS & Workshop

Mobility and Transportation Policy

The value of walking as a viable alternative is indicated by the high rating of the image to the immediate right, the highest rated image in the VPS. Walking as a mobility alternative is already in practice in Newburgh. Unlike the majority of American cities and towns, a sizable percentage of 27% of participants typically walk to travel along Liberty and Grand streets, while 69% of participants in the survey use a car or truck to travel along the corridor.

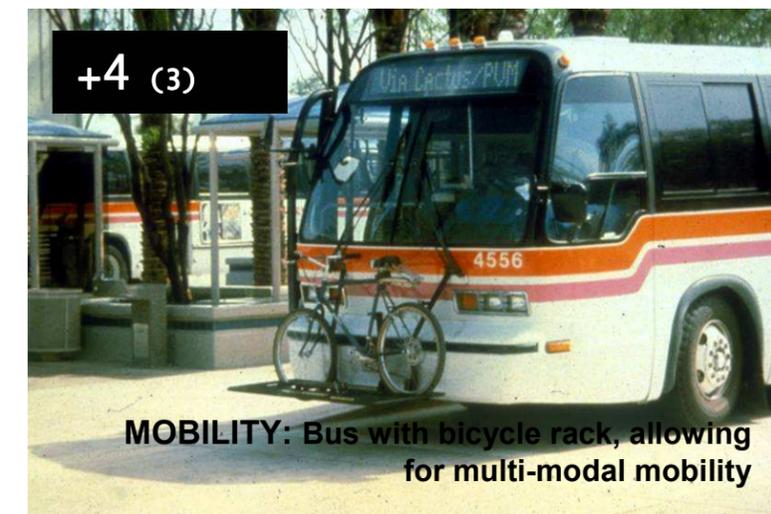
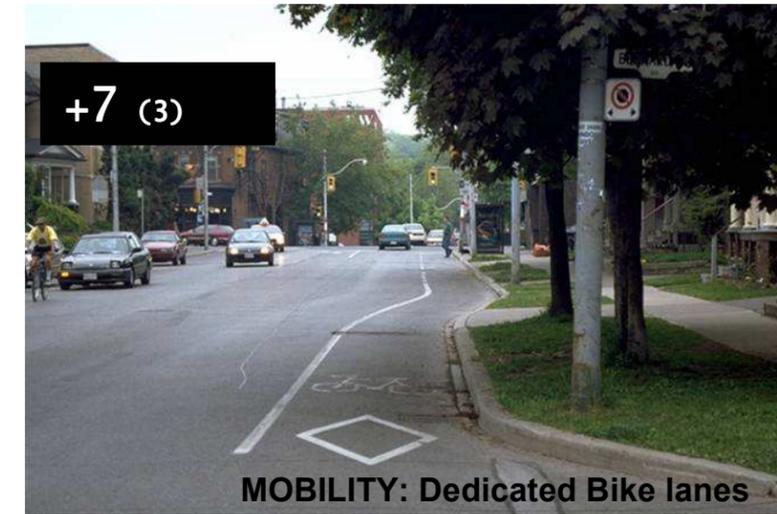
Generally, traffic flow and congestion are not seen as issues affecting the corridor. 47% of participants consider the traffic flow through the corridor as adequate at most times while 19% of participants do not consider traffic flow to be an issue at all along the corridor.

Related to traffic flow along the corridor is the traffic pattern and the timing of traffic lights. While traffic flow is not generally considered an issue, traffic light periods are timed for larger traffic volumes. 46% of participants agree that the timing of traffic lights along Liberty and Grand Streets should be examined as to whether they appropriately reflect traffic patterns along the corridor. Conversely, 41% of participants are undecided as to whether there was a need to examine the timing of traffic lights along the corridor.

While traffic was not identified as a significant issue by most participants, participants consider walking and other mobility alternatives as highly appropriate for the corridor, as indicated by the high image ratings of the mobility alternatives on this page.

Participants are very supportive of improvements made to accommodate and facilitate bicycle ridership along the corridor. 71% of participants either recommend or highly recommend that the City incorporates a comprehensive bicycle network, including bike lanes and paths, connecting the Liberty Street and Grand Street corridor with other locations in the City. In addition, 21% of participants recommend a comprehensive bicycle network in certain locations along the corridor connecting to other locations in the City.

A transportation alternative known as “on-demand” transit, also called “computer commuter” transit, was also highly supported by participants. On-demand transit, illustrated in the middle right image above, uses GPS (Global Positioning Satellite) technology, the same technology which is used to track UPS and Federal Express packages, to dispatch the closest transit van to designated pick-up locations. Along with the high rating of the on-demand transit image above, 70% of participants would either support or strongly support an on-demand transit service set up in Newburgh that would also service the Liberty and Grand Street corridors. Additionally, participants were nearly evenly divided among those who would use on-demand transit every day, those who would use it several times a week, those who would use it several times a month, those who would use it for special occasions and those who would never use on-demand transit.



Liberty – Grand Street Heritage Corridor VPS & Workshop



The “Walking” image to the left was the highest rated image in the Newburgh Visual Preference Survey. 27% of participants currently use walking as their primary mobility option along the corridor.

71% of participants either recommend or highly recommend that the City incorporates a comprehensive bicycle network, including bike lanes and paths, connecting the Liberty Street and Grand Street corridor with other locations in the City

70% would support an “on-demand” transit/limo service and 38% would use it every day or a few times per week.



What the People Want: Policies

Mobility Alternatives

Make walking the top mobility priority

Create a bicycle network.

Create an On-Demand Transit Network , a “Computer Commuter” – GPS guided, point-to-point local transit service.

Improve the public transit bus network and coordinate routes with an on-demand transit network.



Existing Conditions Synthesis Map of the Liberty – Grand Street Heritage Corridor

Participants identified numerous places of interest and historical and heritage sites along the Liberty – Grand Street Corridor, indicated on the map with yellow dots.

Existing bus stops were identified with blue stars. It appears that the northern half of the corridor is not as well served by public transit as is the southern portion of the corridor and Broadway.

Numerous intersections along the corridor were identified as being dangerous, either for vehicular traffic or pedestrians, as indicated by the red circles. Many of the dangerous intersections are located along Liberty Street. In addition, all intersections with Leroy Place were indicated as being dangerous.

Participants perceived the adjacent neighborhoods located to the west of the corridor as having problems with crime, as indicated with the gray cross pattern. The corridor itself, however, was not perceived by participants to have significant issues with crime.

Sidewalks along the entire lengths of both Liberty and Grand streets, as well as a vast majority of sidewalks along adjacent and perpendicular streets, were identified as being in poor or deteriorated condition, as indicated with the red lines.

Excessive vehicular speed and/or noise was noted along the entire length of Liberty Street and in other areas of the corridor, as indicated with the dashed purple lines.



Susceptibility to Change Map of the Liberty – Grand Street Heritage Corridor

The potential for change of each parcel and building based on its condition and susceptibility to be redeveloped, as perceived by workshop participants.

Much of the Liberty Street corridor is identified as being overall more highly susceptible to change in the future. The Grand Street corridor is identified as being overall more moderately or having lower susceptibility to change.

Susceptibility to Change analysis uses the following color code:

HIGH SUSCEPTIBILITY: RED

Single story, deteriorated, vacant, underutilized parcels and/or land uses likely for redevelopment.

MODERATE SUSCEPTIBILITY: ORANGE

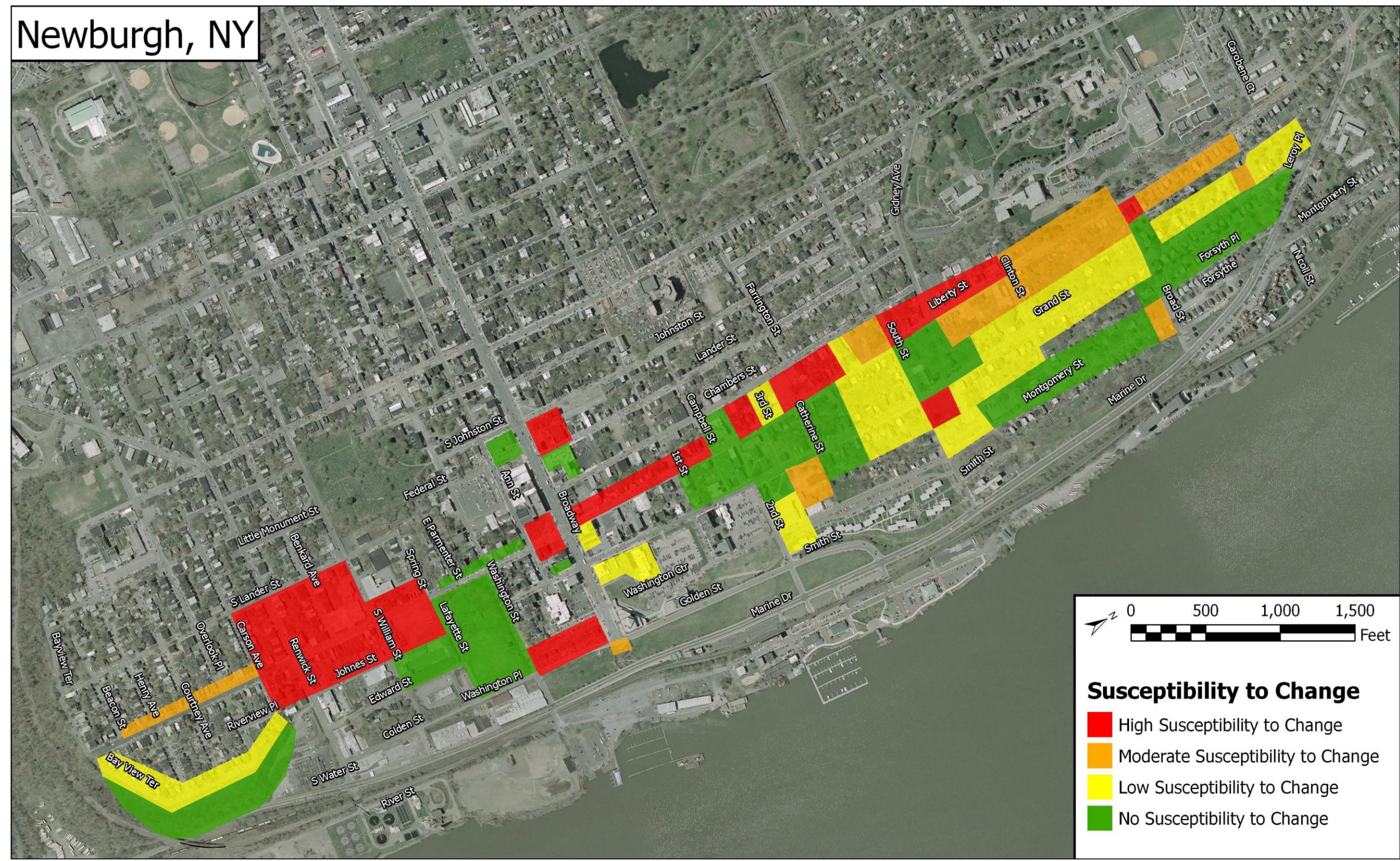
Deteriorated buildings and lots that may have some utility but need significant improvement.

LOW SUSCEPTIBILITY: YELLOW

Buildings and parcels in good or very good condition, that are new and/or that have historical value. No or minimum revitalization.

NO/ VERY LOW SUSCEPTIBILITY GREEN

Land, parcels or buildings in very good to excellent condition, are new and/or have environmental and/or historical value or constraints. No or minimum revitalization.



Recommended Streetscape Map of the Liberty – Grand Street Heritage Corridor

Participants recommended a number of new bus stops primarily along the northern portion of the corridor, indicated with the pale blue circles.

Crosswalk improvements are primarily recommended at intersections of Liberty and Grand streets with other major streets along the corridor., as indicated by the yellow circles with red cross hatching.

Parallel parking is recommended along the entire lengths of both Liberty and Grand streets, as well as a number of adjacent streets, as indicated with the dashed blue lines.

Bicycle lanes are recommended along the entire lengths of Liberty and Grand streets as well as on Broadway and Leroy Place. In addition, bike lanes are recommended on select streets in neighborhoods adjacent to the corridor. Bike lanes are indicated with purple lines.

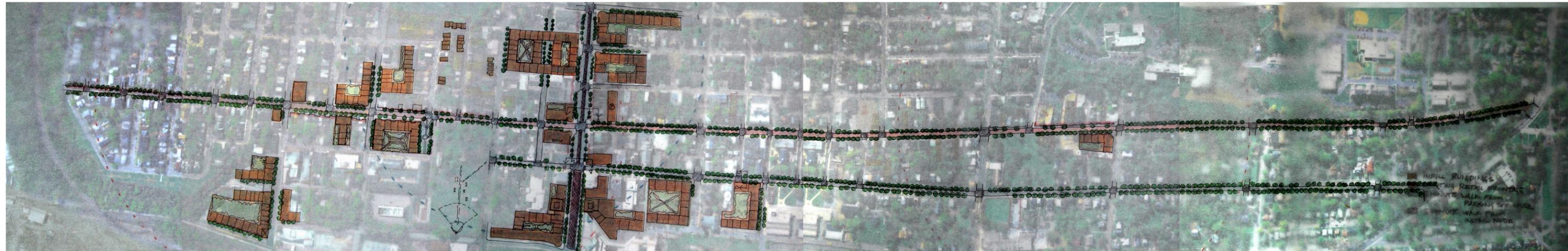
Street lighting improvements are recommended for the entire length of both Liberty and Grand streets, Broadway, and several adjacent streets, indicated by the yellow lines.

Streetscape improvements, including street trees, are recommended for the majority of Liberty Street, the northern half of Grand Street and select adjacent cross streets, as indicated with the green lines. New or improved green spaces are recommended by the green squares.

Both Liberty and Grand streets as well as all adjacent and perpendicular streets are recommended for resurfacing, as indicated by the red lines.



Vision Plan of the Liberty – Grand Street Heritage Corridor



The final Vision Plan, illustrated on the above map, was developed based on both the results of the Visual Preference Survey and the results and recommendations of the Vision Translation Workshop, along with input from the consultant team and City staff. While the Vision Plan reflects a final vision, implementation of the vision would occur in a series of phases.

Along with the participants' preferences for the initial clean-up of the corridor and further on-going maintenance of the streetscape, the Vision Plan reflects the community's preference for resetting, and replacing where necessary, the brick pavement on Liberty Street. The Vision Plan also calls for intersections along the corridor to be textured with cobblestone and textured crosswalks, both of which will serve to slow traffic down and complete the continuity of the streetscape. Care must be taken to ensure that textured sidewalks comply with ADA specifications.

The Vision Plan also calls for significant re-greening of both Liberty Street and Grand Street. The Vision Plan recommends that new street trees be planted where none are currently located at a distance appropriate for either commercial or residential uses. The Vision Plan also recommends the on-going care and maintenance of existing trees along all streets in the corridor.

The Vision Plan above indicates the location of existing retail

frontages along the corridor. New, mixed-use infill buildings are recommended in select locations along and adjacent to the corridor, generally adjacent to or replacing existing marginalized or deteriorated mixed-use buildings with retail frontages. Infill is primarily recommended in locations along the corridor where development has traditionally been more intensive and higher density.

The majority of new infill is recommended adjacent to the intersections of Liberty Street and Grand Street with primary frontages on Broadway. The recommended infill at Ann Street would incorporate a mixed-use parking structure with civic, institutional and commercial uses. As indicated by the blue line, a significant portion of the new recommended infill development is located within a 5 minute walk from this structure, thereby indicating its proximity to a significant portion of the corridor.

Additional infill is located on Renwick Street, at the intersection of Liberty Street and South William Street, Grand Street adjacent to 1st and 2nd Streets and Liberty Street at Clinton Street. As indicated by the red line, a significant portion of the new recommended infill development is also located within a 5 minute walk from the intersections of Broadway and Liberty Street and Grand Street. This indicates that the majority of recommended infill is located within a comfortable walking distance from the primary retail node of the corridor.

As a component of the streetscape, locating new urban plazas has been recommended in the Vision Plan. A primary new urban plaza has been recommended on Broadway between Grand Street and Washington Place, by turning the street to a brick pavement and adding a landscaped median. This would serve to extend the pedestrian experience from the Liberty and Grand Street Corridor down to the riverfront, thereby tying the two areas together. Extending the landscaped median further west along Broadway is also recommended in the Vision Plan as a means of extending the pedestrian experience and improving the streetscapes along the corridor.

Increased visibility and tourism-related activities is one of the primary goals identified in the Vision Plan for the Liberty – Grand Street Heritage Corridor. To facilitate increased tourism and other related activities it is recommended that the corridor capitalize on and improve existing historic sites and properties. The Vision Plan recommends that the Washington Headquarters site expand to become a cultural center with a feature such as an amphitheater at this location, which would take advantage of the historic nature of the site and its views of the Hudson River. This is further discussed on the following page.

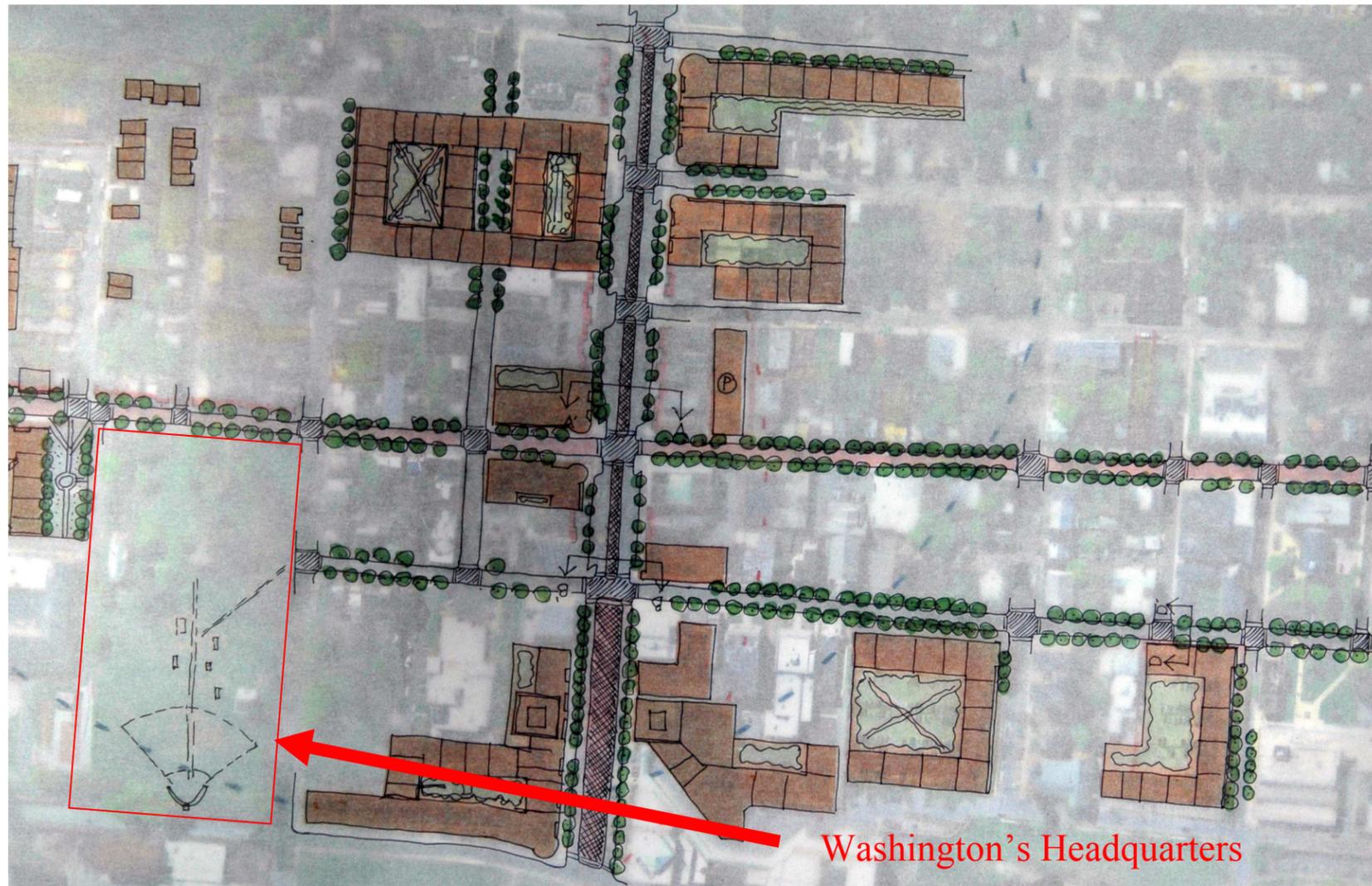
	Infill Buildings
	Existing Retail Frontage
	Brick Street Texture
	Textured Intersection/ Crosswalk
	Urban Plazas
	New Street Trees
	Amphitheater
	5 Minute Walk from Parking Structure
	5 Minute Walk from Retail Node

General Recommendations to increase Public Exposure to the Liberty – Grand Street Heritage Corridor

Intensify the use of Washington's HQs Historic Site

Add amphitheater on east side of property

Increase the number of events happening on the site



Increase Heritage Tourism

- Develop an attractive logo to brand & unify Newburgh's tourism resources
- Develop interpretive plan which includes site locations for appropriate interpretive signs
- Create walking tour brochures that enhance the visitor's (& resident's) appreciation of Newburgh's cultural resources
- Identify special interests tours & outline "bite-size" outings (1-3 hours in length)

Create More Destinations

- Strengthen retail nodes to establish critical mass attractive to residents, adjacent neighbors and visitors
- Promote Empire Zone benefits to attract entrepreneurs to the Corridor
- Encourage the development of diverse restaurants

Funding

- Utilize City's bonding capacity
- Establish public – private partnerships where developers pay lion's share of public improvements within the Corridor
- Utilize Tax Increment Financing mechanism

The Future is not something we
predict, it is something we
create.